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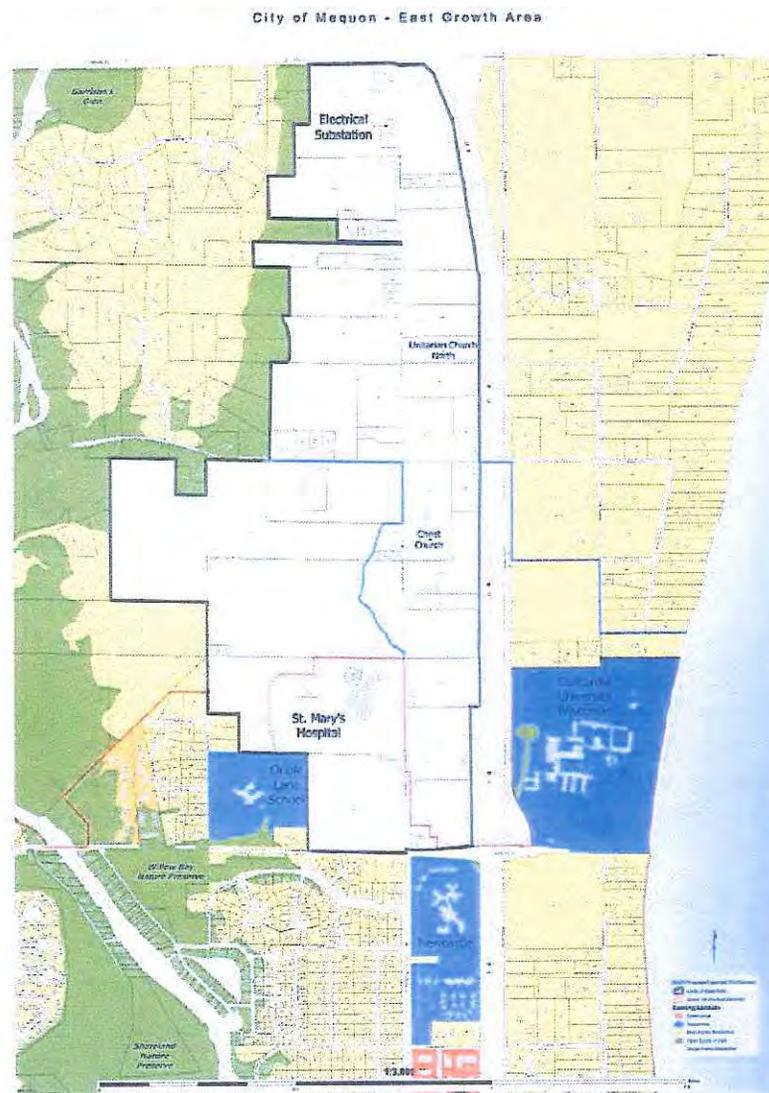
East Growth Neighborhood Land Use Analysis

The East Growth Land Use Committee was created in April of 2012 by the City of Mequon Common Council after a February presentation by Planning staff suggesting land use changes. The purpose of the Committee is as follows (see Resolution in Exhibit A):

Purpose:

- The East Growth Area Land Use Committee shall work in conjunction with staff, the Planning Commission, property owners and the general citizenry of the community in making recommendations to the Common Council related to the land use, zoning, densities and infrastructure of the general area surrounding North Port Washington Road from Highland Road to Pioneer Road.

The Committee's meetings which included a tour of the study area and an Open House with property owners within and immediately surrounding the study area. Committee meetings were well attended by various property owners as well as other stakeholders. Additional actions by the Committee included a survey of the owners and individual meetings between staff and property owners.



The East Growth neighborhood is considered one of three potential growth areas in the City where the opportunity to introduce a range of single-family housing types exists and where there is opportunity to add a significant number of homes. The East Growth neighborhood, unlike the other two potential growth areas, provides opportunities for land uses other than single-family residential. It presents opportunities for commercial, institutional and multiple-family residential.

Background:

The City initiated a Housing Policy Analysis in winter of 2011 which identified three potential Growth Areas, along with other potential sites where an opportunity for residential development at lot sizes less than 5 acres may exist. The discussion was initiated for the following reasons:

- An Economic Development Summit held by the Council in January 2011 suggested to staff the need to review local housing policies.
- The City recognizes there is a recent shift in the housing market. These trends include smaller, energy efficient homes constructed of high quality materials and interior amenities. The trend also includes the need for housing that attracts younger families looking to build their first home, as well as families in all phases of life-transition such as empty-nesters wanting to downsize, students wanting to be closer to school and young professionals looking for a suburban lifestyle.
- It has been over 15 years since broad housing policies were studied. The 2035 Comprehensive Plan for the City of Mequon was approved in 2009 however; the land use plan reflects the status quo related to past housing and land use policy decisions. Prior to the

analysis conducted for the Comprehensive Plan, the most recent analysis that led to broad, residential policy changes was in the mid 1990's.

- Local government policies and zoning influence the housing market. Zoning districts and the technical standards related to lot size and density impact the market and land values. In addition, design standards reflect a desired character and affect the cost of new home construction and home remodeling.
- Residents create demand for goods and services and provide the city with a new workforce pool, community vibrancy and economic vitality.
- There is a growing constituency interested in land use policy decisions. In May of 2011, a "Community Conversation" about the future of Mequon-Thiensville with the Common Council of the City of Mequon, the Village Board of the Village of Thiensville, the School Board of the Mequon-Thiensville School District, and the Board of the Mequon-Thiensville Chamber of Commerce was held. The Community Conversation included an opportunity for the community to express their needs and raise questions. The stated purposes of the discussions were:
 1. To create a forum for the community governance groups to share information about the current state of the City of Mequon, the Village of Thiensville, and the school district and businesses that serve them.
 2. To discuss issues having a long-term impact on the city and village.

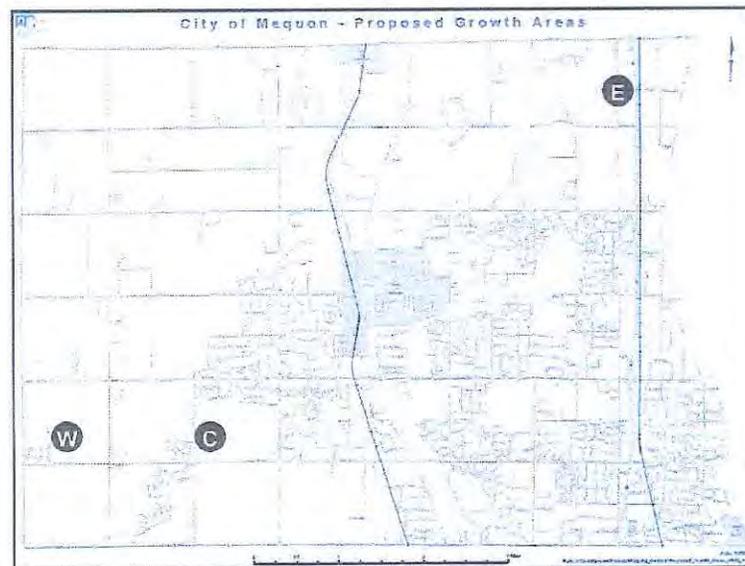
In October of 2011, a Task Force submitted a report with recommendations to the sponsoring agencies. Recommendations centered on quality of life and identity, education, housing and economic development (complete information about the full process, all meetings and the final report and recommendations can be found at www.ci.mequon.wi.us).

The Growth Areas, as identified in the Housing Policy Analysis report dated November 2011, include the West Growth Area (W), Central Growth Area (C) and East Growth area (E) - (see map below). These areas are adjacent to the existing sewer service area, near activity centers – Port Washington Road (in and around Mequon Road) and Town Center have easy access to primary arterials and commuter routes and therefore are within close proximity to job centers and schools. These neighborhoods provide an opportunity to encourage single-family housing development for people that have a great deal of choice related to housing. This housing can target empty nesters, adults with no children and families with children.

Benefits to developing the growth areas include the following:

- Promote housing choice by filling a niche in the housing market that isn't currently being served in the City which includes new single-family home construction on lot sizes less than 5 acres and multiple-family units.
- Stimulate and allow for strategic, controlled growth that will add vibrancy and economic benefits to the community.
- Maintain and enhance property values and aesthetics.
- Generate new tax base.

- Accommodate recent trends in new home construction.



In November of 2011, the Common Council prioritized the Central and East growth areas as areas of focus, leaving the West Growth area for a future discussion.

East Growth Neighborhood Character:

The neighborhood serves as the northern gateway to the City at Port Washington Road and Pioneer Road. At the north, there is an interchange at I-43. The southern end of the neighborhood includes a mix of institutional uses including New Castle Senior Living community, Concordia University of Wisconsin, Oriole Lane School and Columbia-St. Mary's hospital. The neighborhood includes a mix of uses including single-family homes, institutional uses such as churches and

medical services, outdoor recreational facilities and landscape contracting businesses.

The maps in Exhibit B reflect the current zoning and planned land use of the study area. From a land use perspective, the area breaks down as follows:

Land Use	Acres
CRITICAL ENVIRONMENTAL	249.03
INSTITUTIONAL	46.06
OFFICE	2.27
RESIDENTIAL 1-1.5 ACRES	37.91
RESIDENTIAL 5 ACRE MINIMUM	412.86
TOTAL ACRES	748.12

The study area is comprised of 748 acres and 57 parcels which range from .40 to 80 acres in size. A significant area (33%) of the neighborhood is undevelopable due to critical environmental features including Ulao Creek and its associated watershed. The area includes floodplains, wetlands, isolated natural resources and primary and secondary environmental corridors (see Exhibit in C). In addition to these protected lands, the neighborhood is near Garrison Glen, a city preserve at the north end, Oriole Lane School and its associated open space at the south end and at the center of neighborhood, along Bonniwell Road, a site owned and operated by Wisconsin Department of Natural Resources. Each of these sites offer recreational features and activities including a canoe launch to the Milwaukee River, walking

paths, access to small parking areas, signage, passive uses such as bird watching and playground equipment and open fields.

Of the developable area, 82% (613 acres) is currently zoned R-1 (Rural Residential) requiring 5-ac minimum lot size. According to the Zoning Code, Rural Residential neighborhoods “provide for large lot single-family detached residential development in rural areas that are not served by public water or sewerage facilities and that are not yet appropriate for development at higher densities.”

From 1963 through the mid 1990’s, the majority of the land was zoned mostly as a residential neighborhood allowing homes on 1.5 acre lots (R-2: Country Home 1.5 acres). The properties located at the major intersections within the study area were zoned to include a Highway Overlay district. Those intersections include Port Washington and Pioneer Road (west side only), Port Washington and Bonniwell Road (west side only) and at Port Washington and Highland Road (west and east side). The Highway Overlay district allowed for uses such as hotels/motels, restaurants, tourist homes, public and private commercial and non-commercial recreational facilities, commercial entertainment facilities, gas stations, auto repair, outdoor theatres, trailer camps, roadside retail stands.

Based on information provided by one of the property owners, American Indians were present in the area and utilized the Milwaukee River and forest as a lodging area during the warm summer months. The woodlands were harvested, but since reforestation did not occur, the area became actively farmed. Bonniwell Road served as a travel route to Lake Michigan for shipping the harvested wood. Because of the lake and the rail station located at Bonniwell Road and Lake Shore Drive,

Bonniwell Road also hosted a restaurant and bar. This area generally in and around Bonniwell Road was a fairly active trade and travel area.

Issues & Opportunities:

Issues. The Committee has identified the following issues.

1. The area is not best suited as rural in the long term
2. The City has limited opportunities for residential development at lot sizes less than 5 acres.
3. Development in the area is stagnant coupled with an inability to respond to development proposals
4. Soils in the area are not suitable for septic systems

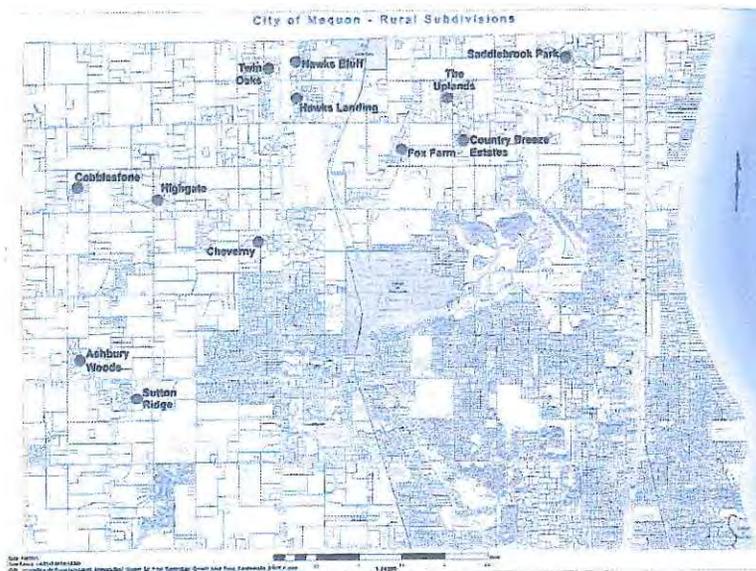
Issue 1: Rural Character. Committee members recognize that while the area may currently appear rural in character, it is not best suited as rural in the long term. The area is located along Port Washington Road, a primary arterial that connects the city to the freeway system and serves as a major activity center. Interstate 43 carries approximately 50,000 vehicles on an average daily basis. The area's proximity to the interstate, and commercial areas along the interstate, as well as the major institutions nearby, suggest a location that can accommodate development beyond single-family residential at 5 acre densities.

In addition, when considering the approximate 10,000 acres of rural zoning per the current Land Use Plan, there are areas better suited to match a homebuyer's interest in a rural residential neighborhood.

The following rural areas have developed at 5 acre densities in the past 10 years. These neighborhoods include the following:

- Cobblestone Woods
- Highgate
- Sutton Ridge
- Ashbury Woods
- Fox Farms
- The Uplands
- Country Breeze
- Cheverny
- Saddlebrook Park
- Hawks Bluff
- Hawks Landing
- Twin Oaks

Of these subdivisions, there are 161 platted at 5 acre densities within the rural community. Of these platted lots, 91 are vacant, resulting in almost 60% of our rural residential platted neighborhoods remaining undeveloped.



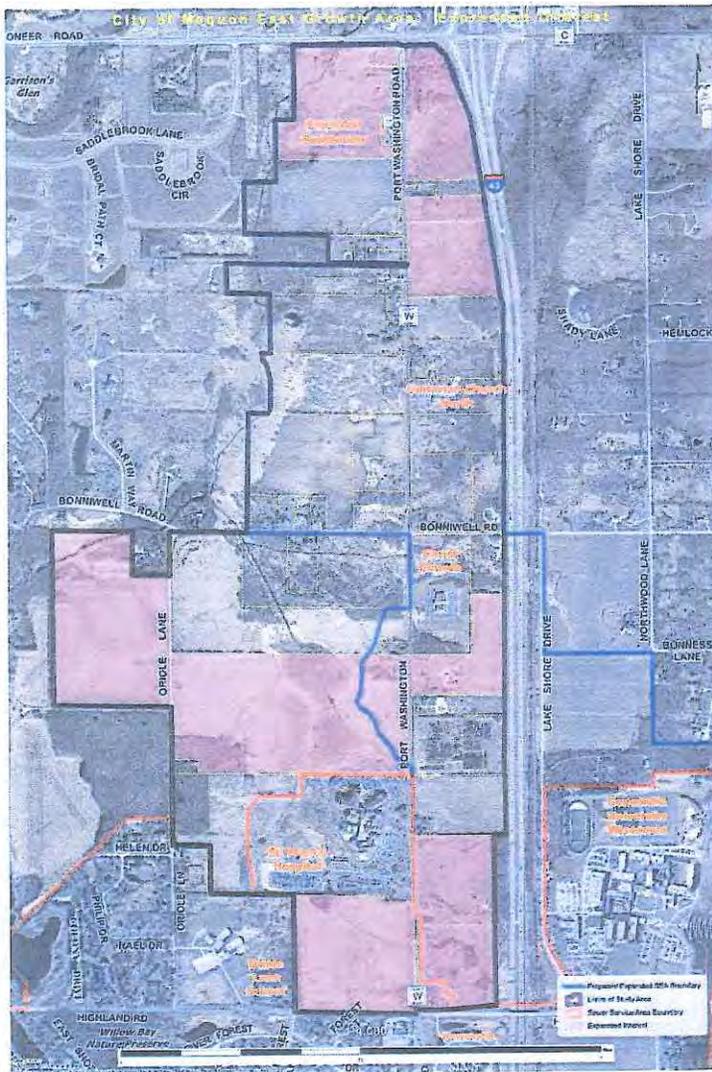
Because of this neighborhood's proximity to I-43 and its associated commercial areas, the saturation of platted rural lots and the vast amount of undeveloped and more appealing rural locations in the City, the Committee finds the current zoning of R-1 limiting.

Issue 2: Limited Opportunities for Residential Development. As indicated earlier, the Housing Policy analysis identified a number of sites where residential development ranging in lots sizes and where multiple-family residential development may be appropriate. Of all the areas identified, the three growth areas are the most ideal to accommodate single-family housing at less than 1 acre because of the proximity to residential neighborhoods that already exist at these densities and proximity to primary arterials and commercial uses.

There are very few parcels zoned for 3/4 acre and 1 acre that remain vacant in the City (80 acres). Currently, there is one site zoned for non-senior, multiple-family residential (Winding Hollow allowing 40 units on Mequon Road) other than the mixed-use opportunities offered in the Town Center. The multiple-family development that exists in the community dates back to the early 1970s with the most recent, Mequon Trail homes, being constructed in 1990.

Issue 3: Stagnant Development. The development in the study area has been stagnant. Factors that contribute to the stagnant development include the current zoning and the City's inability to respond to development proposals. Property owners have expressed interest in developing sites in the recent years, especially at the intersections of Port Washington and Highland Road and at Pioneer Road, but lacking a broader land use policy discussion, decision makers were unwilling to take action.

The following map highlights owners that have expressed an interest in developing their properties. Some of the owners have made formal proposals to the City.



The limited amount of development that has occurred provides evidence that the property owners and developers are taking advantage of the non-residential uses allowed as conditional uses within the Rural Residential zoning district, furthering the Committee's belief that residential lots on 5 acres may not be appealing or marketable in this location. The limited amount development of that has occurred includes two; new landscape contracting businesses, a new church and expansion of the hospital and an existing church and one home remodel. While these types of non-residential uses can be appropriate in a residential neighborhood, the locational conditions provide an opportunity for development that depends on the access to, and visibility of, the interstate.

Issue 3: Unsuitable Soils. The map in Exhibit D represents the soils prominent in the study area. The Committee consulted with the Ozaukee County Land and Water Management Department because the County, not the City, permits private septic systems. According to the soils maps providing by the County, this area is very limiting.

The lack of public infrastructure, coupled with soils not suitable for septic systems, has required new development to rely on holding tanks and thus limits development opportunities in the long term.

Opportunities. The Committee has identified the following opportunities.

1. Commercial nodes at intersections are viable
2. Provide additional and diverse housing that can serve the neighborhood's existing commercial and institutional uses.

3. Enhance the tax base
4. Provide clear direction for development interest

Opportunity 1: Commercial Nodes. The area presents opportunities for a mix of commercial uses that provide convenient goods and services at the intersections of Pioneer Road (due to the interchange at I-43) and at Highland Road due to the nearby institutional uses including New Castle Senior Living, Columbia-St. Mary's, MATC, Oriole Lane School and Concordia University of Wisconsin.

Both Highland and Pioneer Road connect residential neighborhoods to the commercial corridor of Port Washington Road and accommodate commuter traffic to I-43. The traffic conditions at these intersections are consistent with other well-developed commercial nodes including:

- Pioneer and Cedarburg Road
- County Line and Cedarburg Road
- Mequon Road and Hwy 45
- Port Washington and Brown Deer Road
- Port Washington and Green Tree
- Port Washington and Bender
- Silver Spring and Lake Shore Drive
- Oakland Avenue and Kensington

These nodes host a variety of commercial uses and suggest to the Committee that there is a viable market for additional services at Highland Road and Pioneer Road. In addition to the commercial and institutional uses already present in the immediate area, there are also single-family residential neighborhoods developed on lots less than 1 acre. These include River Forest Park, Riverdale Park and River Oaks.

Because of these conditions, the Committee believes commercial nodes at the intersection are compatible with the surrounding uses and will provide additional convenience and services to residents. Further, the new development brings with it an opportunity to enhance the aesthetics of the gateways into the neighborhood and City.

Opportunity 2: Additional and Diverse Housing. The neighborhood provides an opportunity for a mix of residential uses. This is the only area on the east side of the City where enough land mass exists to accommodate a new single-family neighborhood. Based on interviews with real estate agents and census data related to growth, new home construction is occurring north of the city along the I-43 corridor. The Committee believes the city could be more competitive in the housing market on the east side, along I-43.

The Committee considered the following factors when analyzing the area for residential development opportunities:

- A 28.5% increase in the number of single-family residential homes built in the Town of Grafton and Village of Grafton in and around I-43 based on census data from 2000-2010.
- During the 1970s – mid 2000s in the City of Mequon's east side, along the I-43 corridor, single-family residential growth occurred. These neighborhoods, like La Cresta, San Marino and Shady Lane, have an average of 2200-3000 square foot homes valued at \$260,000-\$360,000.
- An evaluation of southeastern Wisconsin communities where new residential development has occurred along Interstates and highways including Brookfield,

Pewaukee and Delafield (I-94), West Bend and Richfield (Hwy 41/45) and New Berlin (I-43) shows that the average home size is 1,900 – 4,100 square feet and a value range of \$280,000-\$410,000.

- Representatives from the area’s institutions identified significant desire from their workforces and clients for nearby housing opportunities.
- Planning staff has responded to a number of concept plans for multiple family projects at various locations throughout the City in the past six months. These projects are suggesting density in the range of 12-15 units/acre and have a combined total of 1550 dwelling units.

These factors suggest that there is interest by homebuyers along freeway corridors and that there is a demand for multiple- family development. In addition to the perceived interest, considerable value can be achieved through additional residential development. A single-family home that ranges in value of \$260-410k can generate an annual tax base of \$4,000 – \$6,000.

The Committee reached out to the Town of Grafton when considering the surrounding zoning and potential uses. The Town of Grafton’s zoning and land use planning suggests that single-family residential development at 1 acre* and commercial uses, such as lower intensity retail and office, restaurants, day care facilities, bed and breakfast operations, indoor recreational facilities and craft manufacturing, will be allowed along the Port Washington Road corridor within the town. The Town has recognized opportunities for new residential and commercial growth along the freeway corridor and intends to implement the necessary changes.

*The Town of Grafton plans to amend all of their current residential zoning districts that require a 3 acre minimum to a 1 acre minimum. The Town anticipates initiating this process early in 2013.

The Committee also compared Mequon’s recent demographic trends and new residential construction size and values to those in similar communities in the area. The following highlights that data:

Demographic Trends.

Table 1:

POPULATION CHARACTERISTICS 1980-2010

Area	1980-1990		1990-2000		2000-2010		1980-2010	
	Number	Percent	Number	Percent	Number	Percent	Number	Percent
Mequon	2,691	16.62%	3,758	19.90%	489	2.16%	6,938	42.84%
Wisconsin	186,002	3.95%	471,939	9.65%	323,278	6.03%	981,219	20.85%
U.S.	22,164,068	9.78%	32,712,033	13.15%	27,323,632	9.71%	82,199,733	36.28%

Source: U.S. Census

POPULATION GROWTH BY PERCENTAGE

	1970-1980	1980-1990	1990-2000	2000-2010
THIENSVILLE	5.00%	-1.00%	-1.43%	-0.58%
DEWAUKEE	41.76%	14.00%	54.53%	-0.05%
RICHFIELD	41.65%	7.19%	15.35%	8.94%
GRAFTON	39.73%	11.44%	12.83%	9.51%
GERMANTOWN	53.84%	27.30%	33.69%	8.15%
CEDARBURG	16.99%	12.00%	10.07%	2.79%
MEQUON	33.28%	16.62%	19.00%	2.16%
WEST BEND	29.77%	13.90%	15.05%	10.39%
MENOMONEE FALLS	-12.15%	-3.61%	21.64%	9.12%
BROOKFIELD	7.16%	3.38%	9.85%	-1.89%
OZAUKEE COUNTY	25.49%	11.37%	13.75%	4.95%

Table 3:

AGE COMPOSITION OF THE POPULATION: 2010

Age Group	Mequon		Wisconsin		United States	
	Number	Percentage	Number	Percentage	Number	Percentage
Under 5	889	4.3%	358,443	6.4%	20,201,382	6.5%
5-9	1,454	6.3%	348,817	7.1%	20,348,657	6.6%
10-14	1,704	7.4%	375,527	7.6%	20,877,194	6.7%
15-19	2,118	9.1%	399,209	7.6%	22,040,343	7.1%
20-24	1,211	5.2%	383,552	6.7%	21,585,999	7.0%
25-24	1,219	5.3%	721,894	13.2%	41,083,348	13.3%
35-44	2,559	11.1%	725,666	16.3%	41,070,606	13.3%
45-54	4,121	17.8%	873,753	13.7%	45,006,716	14.8%
55-64	2,059	8.9%	385,586	4.7%	19,664,805	6.4%
65-64	1,697	7.3%	313,825	3.8%	16,817,524	5.4%
65 and over	4,003	17.3%	777,314	13.7%	40,257,594	13.0%
Total	23132	100%	5,688,588	100%	308,745,538	100%
Median Age	45.9		38.5		37.2	

Source: DP-1 Profile of General Population and 2010 Demographic Profile Data from U.S. Census

AGE COMPOSITION OF THE POPULATION: 2010

Age Group	1990		2000		2010	
	Number	Percentage	Number	Percentage	Number	Percentage
Under 5	1,558	8.8%	1,245	5.5%	989	4.3%
5-14	2,541	13.5%	3,895	17.2%	3,158	13.7%
15-19	1,431	7.6%	1,653	7.3%	2,118	9.1%
20-24	898	5.3%	521	2.3%	1,211	5.2%
25-24	2,052	10.9%	1,359	6.0%	1,219	5.3%
35-44	3,578	19.0%	3,827	16.9%	2,559	11.1%
45-54	2,560	13.6%	4,370	19.3%	4,121	17.8%
55-64	1,958	10.4%	2,595	11.9%	3,756	16.2%
65 and over	2,052	10.9%	3,078	13.6%	4,003	17.3%
Total	18,823	100%	22,643	100%	23132	100%
Median Age	37.1		42.5		45.9	

Source: DP-1 Profile of General Population and 2010 Demographic Profile Data from U.S. Census

The City's population growth in the past decade was less than 500 people. This represents a significant slowdown in growth. In addition, the City is aging. In 1990, the median age was 37, now is 2010, the median age is 46.

Residential Construction.

**Dwelling Units Constructed (by Decade):
City of Mequon**

Year	Single-Family	Plex	Multi-Family (includes senior housing)	Total
1970-1979	1301	332	360	1993
1980-1989	1073	302	91	1466
1990-1999	1251	161	350	1762
2000-2011	582	236	324	1142

Total	4207	1031	1125	6363
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Dwelling Units Constructed (by Year and Type): City of Mequon

Year	Single-Family	Plex	Multi-Family (includes senior housing)	Total
2005	60	10	0	70
2006	64	18	0	82
2007	50	16	0	66
2008	25	8	0	33
2009	19	6	0	25
2010	20	4	0	25
2011	23	4	0	27
Total	4207	1031	1125	6414

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New Housing Construction Size and Values for 1 and 2 Family Dwellings 2000-2010:

	<u>Sq.ft.</u>	<u>\$</u>
• Mequon	4568	535k
• Grafton	2800	225k
• Germantown	2800	280-350k
• Brookfield	4000	435k
• Cedarburg	3,000	285k
• Hartland	3,600	410k
• Menomonee Falls	3,000	285k
• Richfield	2,800	305k
• New Berlin	3,000	300k
• Franklin	2,600	235k

There has been a slowdown in new homes constructed and based on the averages above, Mequon home starts in the past 10 years are significantly larger in size and in value than compared to other communities in SE Wisconsin. This is, in part, driven by zoning and density, land values and design standards.

The City currently provides opportunities for multiple-family residential development within mixed-use developments in the Town Center. Other than sites that allow for senior housing facilities, the City has approved only one new multiple family housing development (40 total units at Winding Hollow located on Mequon Road) City in the past 10 years.

Additional residential units in the neighborhood increase the overall density in and around the existing Port Washington Road Commercial Corridor. The additional growth benefits our local businesses because there is new demand and dollars to be spent on goods and services. When comparing the City's density levels in and around our commercial corridors to other communities, Mequon is more than 50% lower than our neighboring community of Cedarburg. Low density levels have an impact on attracting certain types of commercial operations that may be suited for Mequon's current commercial mix and could assist in creating the synergy for new commercial uses. These types of uses include additional casual, family restaurants and mid-size retailers such as sporting good retailers and booksellers.

Density @ 1 mile

• Mequon	.66 du
• Cedarburg	1.6 du
• Whitefish Bay	3.2 du
• Shorewood	5.3 du

While these comparisons are more urban in nature, the uses present at these locations tend to be the type of lower intensity retail and services desired by the City in contrast to the larger, more regional commercial areas of Brookfield, Glendale and Grafton at Hwy 60.

Land Use Options

Methodology and Considerations for Land Use Options:

After exploring the neighborhood's history, current characteristics and the issues and opportunities, the Committee then sought input from property owners, affected neighbors and the Common Council, in an effort to generate considerations for land use options.

Property Owners. On August 22nd, an Open House was well attended by Committee members and property owners (22 owners present of 54). Property owners were asked to complete a number of exercises including:

1. **Team Exercise.** A team exercise in which members were asked to envision the future growth of the corridor and assign land use categories based on a series of questions (for a complete instructional sheet see Exhibit E).

The land use mapping exercise generated considerable consistency within the study area. Five team concept maps

were generated by the participants and resulted in some general consensus in the following geographic areas (see team maps labeled Orioles, Highlanders, Chinooks, Spartans and Wildcats within Exhibit H):

- Commercial and industrial uses in and around Pioneer Road.
 - Commercial and multiple family uses in and around Highland Road.
 - Conventional residential on Oriole Lane.
 - Residential uses varying between single-family, duplex and multiple family through the middle of the corridor at higher densities than recently permitted in the City.
2. **An Opportunity for Individual Comment.** Anonymous comments could be left on a map of the study area. Those comments are listed in Exhibit F).
 3. **Design and Imagery Reactions.** An individual exercise where owners reacted to imagery regarding residential designs (duplex and multiple-family), commercial designs at various sizes and roadway designs (see attached imagery and results in Exhibit G). The reaction to design imagery suggests the following:
 - A landscaped, wider boulevard roadway design is preferred.
 - A mix of tradition and contemporary designs with greater architectural detail is preferred. Modern (very contemporary) architecture and building materials did not rate well.
 4. **Open dialogue and Brainstorming.** As a summary to the open house, attendees had open dialogue about all of the results and brainstormed on additional ideas (see attached comments in meeting minutes from Open House).

Property Owners Meetings. Prior to finalizing the committee's work, staff conducted one-on-one meetings with property owners within and adjacent to the study area. A number of property owners immediately adjacent to the study area that staff met with were not opposed to the land use concepts allowing for growth even on properties directly adjacent to their homes. Some questioned the amount of commercial/industrial along the east side of the corridor and expressed concern about an increase in truck traffic and aesthetics of the type of industrial use. A few opposed the change from a rural character. Staff recommended to the owners that they attend the meeting and provide any commentary, including in written form, prior to the final report.

All but one of the property owners that staff met with that would have the land use of their property change seemed supportive. One of those property owners did not object to the change, but did want the right to continue their current commercial operations on the property.

Common Council. In September, the Common Council was updated on the work of the committee and provided direction on the remaining analysis. In October, the Common Council completed the same land use planning exercise that the property owners completed. The Council's mapping exercise resulted in similar concepts to those presented by the property owners. (see Exhibit I).

Consideration of Possible and Desired Uses. As a result of the discussions centering on land use concepts, a list of potential uses were generated. Those uses are as listed in Exhibit J.

Technical Review for Land Use Options:

In order to generate land use concepts, the Committee also considered the existing infrastructure, transportation and road conditions, critical environmental features, open space and recreational features and connectivity opportunities. The following highlights the existing conditions for the study area.

Transportation and Road Conditions.

According to 2010 Wisconsin Department of Transportation traffic counts, the annual average daily traffic between Mequon Road and Highland Road is 9800 and between Highland Road and Pioneer Road is 7900. This is based upon actual traffic counts for cars traveling on Port Washington Road and does not include traffic projections for potential development on undeveloped land or redevelopment of underutilized land.

Port Washington Road is under the jurisdiction of the Ozaukee County Highway Department. Any modifications to the cross section to accommodate increased traffic volumes, or additional intersection controls would require approval and implementation by the County. Typically the plan for improvements would start with a traffic study to determine the projection of traffic volumes due to land use and development and then move forward with design and construction. Analysis would include any requirements for utility relocation, right-of-way and easement acquisition and cost sharing. This process can take several years to implement, depending on the magnitude of the proposed changes.

Sewer and Water Conditions.

With the exception of Columbia St. Mary's and the parcel immediately south, the East Growth Area is outside the City's

sanitary sewer service area (SSSA). Due to topography constraints, CSM is currently being served by a private lift station which discharges through a private forcemain along Port Washington Road into the gravity system at Highland Road. Staff has conducted some preliminary analysis on the extension of the sewer service area as well as our limitations within our existing system.

Since our sanitary sewer system discharges and is treated by the Milwaukee Metropolitan Sewerage District (MMSD), any change to our current sewer service area must be approved by multiple regulatory agencies. The process to amend the SSSA is detailed below:

1. Resolution by Council to request expansion
2. City to Amend Land Use Plan and Rezone properties
3. Southeast Wisconsin Regional Planning Commission(SEWRPC) analysis and Public Information Meeting
4. MMSD Review
5. SEWRPC amendment to Regional Water Quality Management Plan
6. Wisconsin Department of Natural Resources(WDNR) Review
7. Council Action* forwarding SSSA expansion to MMSD. *Per ordinance, the action must be approved by a super majority of the Council(3/4 vote)
8. MMSD Commission Action

The main focus of the review and approval from the various agencies is to ensure that there is adequate capacity within MMSD's system and the impact on any downstream communities. Preliminary modeling of the additional flows from the SSSA expansion predict that the City will exceed the

maximum peak allowable flows into MMSD's system. While this does not restrict the City of expanding the SSSA, any infrastructure plans may require storage or buffering of flows.

In addition, the City will need to further investigate any potential issues with our existing system. A Sanitary Sewer System Evaluation was completed for the City's system in in 2010 which identified two areas of limitation that need to be addressed prior to any SSSA expansion.

- *Lift Station E:* Most of the City's southeast side ultimately flows to the City's largest Lift Station E which is located near Ranch Road and Oriole Lane. This lift station is projected to be operating at capacity with full build-out of the City's current SSSA.
- *East Trunk Sewer:* Two forcemains from Lift Station E ultimately discharge into the City's gravity system in two different locations in the southeastern part of the City. A bottleneck in the current system has been identified and the system has experience surcharging under current loading from wet weather conditions. Under wet weather conditions, the City has experience sanitary sewer overflows in which our sanitary crews must bypass sewerage from the system into the road-side ditch in order to avoid basement back-ups.

Some preliminary estimates have been prepared for the East Trunk Sewer relief project as well as the East Growth Area infrastructure expansion. These costs are very preliminary and will be refined through the preliminary engineering report:

- East Trunk Sewer: Estimate of \$8 to \$10M.
- East Growth Area: Estimate of \$8 million. The main reason for large cost is the need for a lift station and the distance to tie into existing system without taxing Lift Station E. There may be alternatives identified as part

of the East Trunk sewer analysis and design which could decrease this infrastructure cost.

The City's water system service area includes the entire City. Within the East Growth area, there is a looped 16" main that serves CSM which would provide adequate supply if the main was extended north. The estimated cost to install a watermain along Port Washington Road north to Pioneer Road is approximately \$2.5 million.

The water system historically has been expanded as developer build and contribute. The Water Utility does not install main on speculation. Any main extensions are typically development driven and paid by the developer. The Utility's connection rules allow for refund agreements with the developers which allow them to recoup some of the infrastructure costs if other developments connect to the new main within 10 years. For existing structures, the connection to the water system is voluntary.

Unlike the expansion of the sewer service area, there are limited approvals needed to expand the water system. The main approving authority is the Wisconsin Department of Natural Resources (WDNR) which will review the design of the new system to ensure that it meets their standards.

After input from the property owners, policy makers and affected neighbors, and a base understanding of existing conditions, the East Growth Land Use Committee further evaluated all the land use concepts and developed three alternatives. The Committee next looked at traffic and road conditions, sewer and water needs of the three alternative land use concepts were considered:

- A growth plan focused on Single-Family Residential development with limited multiple-family and commercial development (see Exhibit K-1).

- A growth plan focused on Multiple-Family Residential development with limited single-family and commercial development (see Exhibit K-2).
- A growth plan focused on Commercial development with limited single-family and multiple-family development (see Exhibit K-3).

Map:	K-1	K-2	K-3
Single-family On private systems:	69	54	54
Single-family On public systems:	138	52	39
Duplex:	103	331	143
Multiple-family: @ 8 units/acre	594	892	589
@ 15 units/acre	1114	1673	1105
Commercial uses By square footage:	734,522 (66 acres)	670,911 (60 acres)	1,524,643 (137 acres)

Based on an assessment by the Engineering Department, the water and sewer needs of each growth plan are essentially the same due to the necessary sizing of the mains. Traffic projections for the three proposed plans vary significantly and depend heavily on the type and size of residential and commercial development. For the two residentially focused plans, the traffic projections would yield average daily traffic counts similar to that of the Mequon and River Road intersection. For the commercially focused growth plan, depending on the type of commercial development, the commercially focused plans could nearly double.

General, estimated population for each plan is as follows:

Map:	L-1	L-2	L-3
Population	2400	3400	2200

Implementation Strategies

The East Growth Land Use Committee established goals for the preferred land use option as follows:

- Enhance the tax base
- Create new housing opportunities at densities other than one dwelling unit / 5 acre
- Create the framework for a desirable and marketable neighborhood.
- Achieve the highest and best use while remaining compatible to existing uses and true to Mequon's standards.

With the information and goals listed above, specific implementation strategies present themselves as follows.

1. *Land Use/Zoning: Preferred Option*

The Committee presents a land use plan option for the neighborhood that includes a mix of land uses that is generally consistent with the variety of team concepts generated throughout the process (see Preferred Option in Exhibit L). It is important to acknowledge that the Committee believes this land use plan is a 20-30 year vision for the area. The following describes the preferred land use plan:

- *Commercial nodes.* Commercial nodes are recommended at the intersection of Pioneer Road and Highland Road to provide convenience goods and services for residents and commuters. These nodes should provide some retail and provide high end design and gateway features. The committee believed that the node at Highland Road should have uses that maintain an established, neighborhood feel to serve the surrounding commercial, institutional and single-family uses and the new, high density residential uses proposed within the corridor. The Committee also felt that mixed-use development, i.e. allowing residential development, would be appropriate at the commercial nodes. The type of potential uses deemed compatible includes eateries, small scale retail, day cares, health and beauty, professional services and offices.
- *Mix of Commercial and Multiple-Family Residential.* A mix of commercial and multiple-family residential uses are recommended for the southern portion of the neighborhood, along the east side of Port Washington Road. The Committee felt single-family residential was not appropriate on the east side. A mix is shown because the Committee didn't want to make distinctions on a parcel by parcel basis on the east side and because they believed low intensity commercial uses and multiple-family residential can coexist within this block. In addition, the location of the multiple family units was strategic in an effort to supply the demand for multiple-family in close proximity to the existing commercial and institutional uses already

established in the surrounding area. The types of potential uses deemed compatible include technology and research, educational facilities, corporate centers, hotels. Of the three growth area, the East Growth area is the only one deemed appropriate for multiple-family residential.

- *Mix of Commercial and Industrial.* A mix of commercial and industrial uses is recommended north of Bonniwell Road, on the east side. The Committee believes an opportunity for research and development, healthcare centers and campus-like corporate centers are viable in this area. The Committee recommends that the nature of these industrial uses remain clean and of higher value. When comparing industrial uses where the principle operations are manufacturing, warehouse and distribution to those that are corporate and research/development in nature, the corporate centers create two times the value. This distinction was critical to the Committee based on the goal of enhancing the tax base, providing compatibility of uses and the need to expand public utilities. In addition, the location for these types of uses was chosen due to its close proximity to the Pioneer Road interchange to I-43. The Committee believes the close proximity will contain truck traffic to the northern end of the neighborhood.
- *Single-Family Residential.* Single-family residential is recommended for the majority of the west side of the corridor. The Committee recommends lots at less than 1 acre north of Bonniwell Road being served by public

sewer and water. Single-family residential is recommended south of Bonniwell Road, along Oriole Lane, at 1 acre lots served by community well and septic systems. It was important to the Committee to address housing diversity and provide for residential growth. The proposed sites offer amenities associated with the existing natural features of Ulao Creek making it a desirable location and serves as a nice transition between the Port Washington Road corridor and the established residential development west of Ulao Creek. The Committee felt that a new neighborhood with convenient access to I-43 and commercial uses would be marketable.

- *Undetermined.* The Committee agreed to slate two sites as undetermined, due to the variety of opinions generated, and allow the market to dictate. The Committee believes a variety of uses could be established. The undetermined property located at the northwest corner of Port Washington and Highland Road is likely best suited to accommodate some commercial at the immediate corner and along the Port Washington Road street frontage. The Committee believes a mix of residential uses could be served on the remainder of the site or the site could develop as a single-family residential development similar to the neighborhood immediately south of Highland Road. The undetermined property located south of the electrical substation, on the east side of the road, could be established as single-family residential or additional commercial/industrial use.

Preferred Land Use Option:

Land use:	Acreage	
Commercial	24.85	
Commercial/MF	86.83	
Commercial/Ind	76.03	
Multiple Family	11.52*	
*(735 dwelling units when combined with MF in the Commercial category above)		
SF residential – public	67.14	(89 dwelling units)
SF residential – private	105.56	(105 dwelling units)
Undetermined	55.86	

General, estimated population for the recommended plan is as follows:

Map:	Recommended land Use Plan
Population:	1962
Value:	
Residential	\$176,000,000
Commercial	\$97,500,000

Changes to the land use in the East Growth neighborhood as recommended by the Committee results in similar development patterns that already exist in the city and the metro area such as:

- Cedarburg Road: Mequon to County Line Road
- Port Washington Road: Highland Road to Glen Oaks
- Mequon Road: Cedarburg to Port Washington Road

- Mixed Use Nodes at: Pioneer and Cedarburg Road, County Line and Cedarburg Road, Port Washington Road and Green Tree, Oakland Avenue and Kensington (Shorewood), Silver Spring and Lake Shore Drive (Whitefish Bay)

It also results in similar residential neighborhoods that already exist in the community. These established neighborhoods contain moderately sized, well-valued and well-designed homes that appeal to younger families. Examples include Brighton Ridge, Knightsbridge Estates, Huntington Park, Westchester Lakes, Bayberry Fields, Ville du Parc, and River Forest Park, Riverdale Park and River Oaks. These neighborhoods set the tone and provide an example of the type of single-family housing the City could encourage within the growth area. The Committee recommends ¾ acre single-family lots for areas served by public sewer and water and 1 acre lots for single-family lots served by private systems.

Of the three growth areas, the East Growth area is the most ideal to accommodate multiple-family housing given its proximity to the Port Washington Road commercial area and I-43. Allowing multiple-family housing in the East Growth area establishes a similar development pattern to that further south on Port Washington Road which includes Apple Orchard, Greenbriar Estates and Cedar Gables These developments however; are dated. The Committee recommends densities in the 10-15 units/acre range.

The Committee agreed that the higher density proposed for the residential development will assist in keeping the cost of development and ultimately the new home construction cost lower. In addition, the density will assist in offsetting the cost

of infrastructure improvements and allow the development to occur at a quicker pace.

One of the goals of the Committee is to achieve the highest and best use to grow the tax base. The highest and best use can be determined by considering: 1) what is legally permissible through zoning codes and any deed or land restrictions and building codes; 2) what is physically permissible due to size, shape, topography and soil conditions; 3) what is financially feasible by evaluating the cost of development compared to the return on the development, and 4) what is most productive through maximizing the opportunity.

To fully vet out the highest and best use, the Committee acknowledges that additional action steps are necessary. Those implementation strategies are recommended as follows.

1. Complete the Proposed Market Analysis.

In September of 2012, the Council requested staff prepare a Request for Proposals for a market analysis of the East Growth Neighborhood. Both the East Growth Land Use Committee and the Economic Development Board believe a market analysis is necessary to determine what, if any, commercial opportunities are present for the study area. The purpose of the market analysis is as follows:

- Explore the type and amount of commercial uses that are viable for the area. Commercial uses, according to the request, includes retail, restaurant, office, industrial
- Determine if there is a niche in the commercial market that Mequon could provide

- Determine if the neighborhood can support commercial development without undermining the City's existing commercial districts including Port Washington Road – in and around Mequon Road – and Town Center.
- Determine the estimated absorption rate of any commercial opportunity in the short (5 years), mid (6-10 years) and long (10-20 years) range.

The Committee recommends completing the market analysis in an effort to better understand the market reality of expanding commercial uses into the East Growth neighborhood.

2. Address the Sewer Service Expansion Area and Infrastructure.

In order to address the current capacity issues within the City's system, the City has begun the consultant selection process for the alternative analysis and preliminary design for East Trunk Sewer relief/upgrades as well as identifying infrastructure alternatives for serving the expanded SSSA for the East Growth Area. Timing for the preliminary analysis and engineering report is summarized below:

1. Design contract award – January 2013
2. Preliminary Engineering Report Complete – June 2013
3. Final Design and Funding – June 2014
4. Construction – 2014-2016 (highly dependent on right-of-way needs and funding availability).

While some properties in the East Growth area are adjacent to existing infrastructure, new sanitary and water mains are required to serve the entire area. Historically, the City's infrastructure system has grown as a "Developer build-and-

contribute” system in which the developer pays for all off-site and on-site infrastructure needs. In order to spur growth, options may exist for either City or Utility-funded expansions. This option will require extensive analysis as both the City and Utilities have limited capital reserves for such expenditures that are in the millions of dollars range. While options exist for the City/Utilities to potentially borrow and recover these costs through special assessments, impact fees or recapture agreements, there is risk to the City to front the costs. The potential SSSA expansion will require policy discussions which include:

- *Should the City continue with the “Developer build-and-contribute” model?*

While this option represents the least amount of financial risk for the City, properties nearest existing infrastructure will tend to develop first.

- *Should the City/Utility install infrastructure to promote growth? What are the preferred cost recovery methods?*

The potential expansion of the SSSA will require policy discussions on the payment of these charges which include:

- *Who should pay for the MMSD Retroactive Charge if the SSA is expanded?*

Since this MMSD policy was enacted in 2012, there is no current City policy. Charges could be borne by the property owner, City, or Sanitary Utility.

- *When should properties be included in the Sanitary Utility District (SUD)?*

Current City policy is that any property within the SUD pays the annual capital charge associated with that

property. Our current SUD boundary does not match our current SSSA for some properties are excluded since they are not able to be served by sanitary sewer. While the intent of the proposed SSSA expansion is to be able to serve all properties, the infrastructure improvements may be delayed years after the expansion. Properties within the SSSA expansion could be included in the SUD (and thus, incur the annual capital charges) at the time of the SSSA expansion or at the time sewer infrastructure is available.

- *What policy should exist for existing homes that are included in the SSSA for both connection requirements and the capital charges?*

Within the proposed East Growth area, there are a number of existing homes that would be included in the expanded SSSA but still be served by private on-site systems. While the City may require compulsory connection to sewer, the policy discussion needs to be held by the Council on how these properties will be treated.

There are a number of infrastructure design and policy decisions that need to be addressed as final land-use decisions are made and prior to implementation.

3. Open Space and Recreation.

Ulao Creek was quickly identified as a natural resource to be protected and enhanced as a neighborhood benefit. The Committee recommends an open space buffer be established and that a dedicated, public path be required as development occurs. The path will link the larger, community recreational resources of the neighborhood and provide an amenity that

will likely be very valuable to the type of homebuyers the City hopes to attract.

4. *Regional stormwater management.*

It is well known by Committee members, property owners and city staff that there is a history of flooding and drainage issues within the neighborhood particularly in and around Bonniwell Road. The Committee recommends the City consider a regional/neighborhood approach versus a piecemeal approach to stormwater management. The Committee believes a coordinated neighborhood system will alleviate concerns of existing homeowners, guide and streamline future development and further the environmental and open spaces amenities of the neighborhood.

5. *Traffic and Road design & I-43.*

The Committee heard varying opinions about an interchange at Highland Road throughout the process. Some suggest maintaining Highland Road as a neighborhood node in the interest of preserving the character of the neighborhoods west of Port Washington Road. With this opinion came the recommendation to make improvements and enhance the existing interchange at Pioneer Road. Others suggest expanding the opportunities at Highland Road by supporting an interchange. With this opinion came an interest in understanding the broader, more global, benefits to the City's other, major east/west arterial roads, such as Mequon Road and how the interchange may distribute traffic and alleviate some of the perceived pressures at Port Washington and Mequon Road. In addition, the Committee acknowledged the

financial commitment necessary for the improvements. Given the infancy of the planning analysis for I-43 corridor improvements, the Committee felt it was premature to provide a firm recommendation on the need for an interchange at Highland Road.

That being said, based on the Committee's review of some very preliminary concepts for corridor improvements, it appears that very limited additional right-of-way needs to be acquired for improvements along the west side of I-43.

6. *Alternative modes of transportation.*

Included in Exhibit x is the Mequon -Thiensville Bikeway Commission's 2030 Recommendation map. The Committee considered the existing conditions and recommendations. The Committee recommends that as improvements are planned or made to Port Washington Road, the Bikeway Committee's planned improvements for bicyclists and pedestrians are made, including any necessary easements. The Committee also suggested the City consider a pedestrian bridge over I-43 to link the two north residential neighborhoods at a location other than Highland and Pioneer Road, providing a direct link to Concordia University, Lake Shore Drive for biking and running and the lakefront.

7. *Historic Acknowledgment.*

Based on the history of the neighborhood provided by one of the property owners, the Committee recommends acknowledgment of history through signage and by utilizing the DNR site at Bonniwell Road. The Committee believes the significance of the travels and uses along Bonniwell Road as a connection between the Milwaukee River, the rail station and

Lake Michigan tells a unique story about the neighborhood that should not be lost. In addition, because the Committee and the Bikeway Committee recommend improvements that encourage recreational activities, it provides a rest stop and point of interest for pedestrians or bicyclists passing through the area.

8. Zoning and Design standards.

The Committee recommends that staff develop zoning standards that accommodate the type of potential commercial uses mentioned as opportunities throughout the report and as listed in the exhibits. Of particular interest, it is recommended that an overlay district be established for the commercial nodes at each north and south intersection. The overlay should provide unique opportunities for these sites positioned along the interstate. In addition, the Committee recommends the creation of a zoning district that allows certain commercial uses and multiple-family residential development to co-exist. The Committee stressed the importance of allowing the necessary building mass and densities to support development at moderate home construction costs and support the expansion of the necessary infrastructure. Further, there is a continued commitment to high quality materials and design features that contribute to long lasting value. The Committee recommends staff develop these standards by advancing new zoning districts.

9. Phased Approach

The neighborhood can be addresses in a phased approach dependent upon decisions made regarding the expansion of infrastructure for sewer and water and estimated absorption

rates. Phase I to include properties located along Port Washington Road from Highland Road north to Bonniwell Road and Phase II to include properties located along Port Washington Road from Bonniwell Road to Pioneer Road.

Future Actions & Timing

Actions	Timeline
Complete RFP for market analysis	February – April 2013
Amend Land Use & Create Zoning	By June 2013
Sewer Service Expansion	June 2013 – January 2014
Sewer Design	June 2013
Sewer Construction	2014 - 2016

EXHIBIT A

COMMON COUNCIL OF THE CITY OF MEQUON

Resolution No. 3089

CREATE AN EAST GROWTH AREA LAND USE COMMITTEE

WHEREAS, the Mequon Common Council has determined that a land use committee should be established in accordance with the Committee of the Whole discussion for the East Growth Area; and

WHEREAS, it is necessary for an organized body to specifically address the land use and zoning, as well as issues related to infrastructure, of the general area surrounding North Port Washington Road from Highland Road to Pioneer Road.

NOW, THEREFORE, BE IT RESOLVED BY THE COMMON COUNCIL FOR THE CITY OF MEQUON, that a Land Use Committee is hereby created in accordance with the following:

1. **Purpose:** The East Growth Area Land Use Committee shall work in conjunction with staff, the Planning Commission, property owners and the general citizenry of the community in making recommendations to the Common Council related to the land use, zoning, densities and infrastructure of the geographic area as described above.
2. **Appointment and composition of the Committee :**
 - a. The Committee shall consist of seven (7) voting members as described below.
 - b. The Mayor shall make appointments consisting of the following:
 - 1) One aldermanic representative and an alternate alderman.
 - 2) Two Planning Commission representatives.
 - 3) Four members of the general public, one of which shall be an Economic Development Board member.
 - 4) One alternate member of the general public.
 - c. From the seven (7) voting members, one (1) Chair shall be appointed jointly by the members of the East Growth Land Use Committee at its organizational meeting.
3. **Administration:** Upon appointment, the Committee shall immediately assemble and meet. At its organizational meeting, it shall select a Chair and shall make such other organizational decisions as it deems necessary or advisable to best carryout its duties. Minutes and recommendations of the meeting shall be submitted to the Planning Commission and Common Council.
4. **Term of Office and Sunset:** The Term of Office for each member for this Committee shall be 6 months or earlier if the purpose is accomplished.
5. **Quorum:** Four (4) voting members shall constitute a quorum for the transaction of business. A simple majority shall be required for action purposes.

Approved: _____

Curtis Gielow, Mayor

Date Approved: _____

This is to certify that the foregoing resolution was adopted by the City of Mequon at a meeting held on the 17th day of April, 2012.

Lee Szymborski, City Clerk

EXHIBIT B

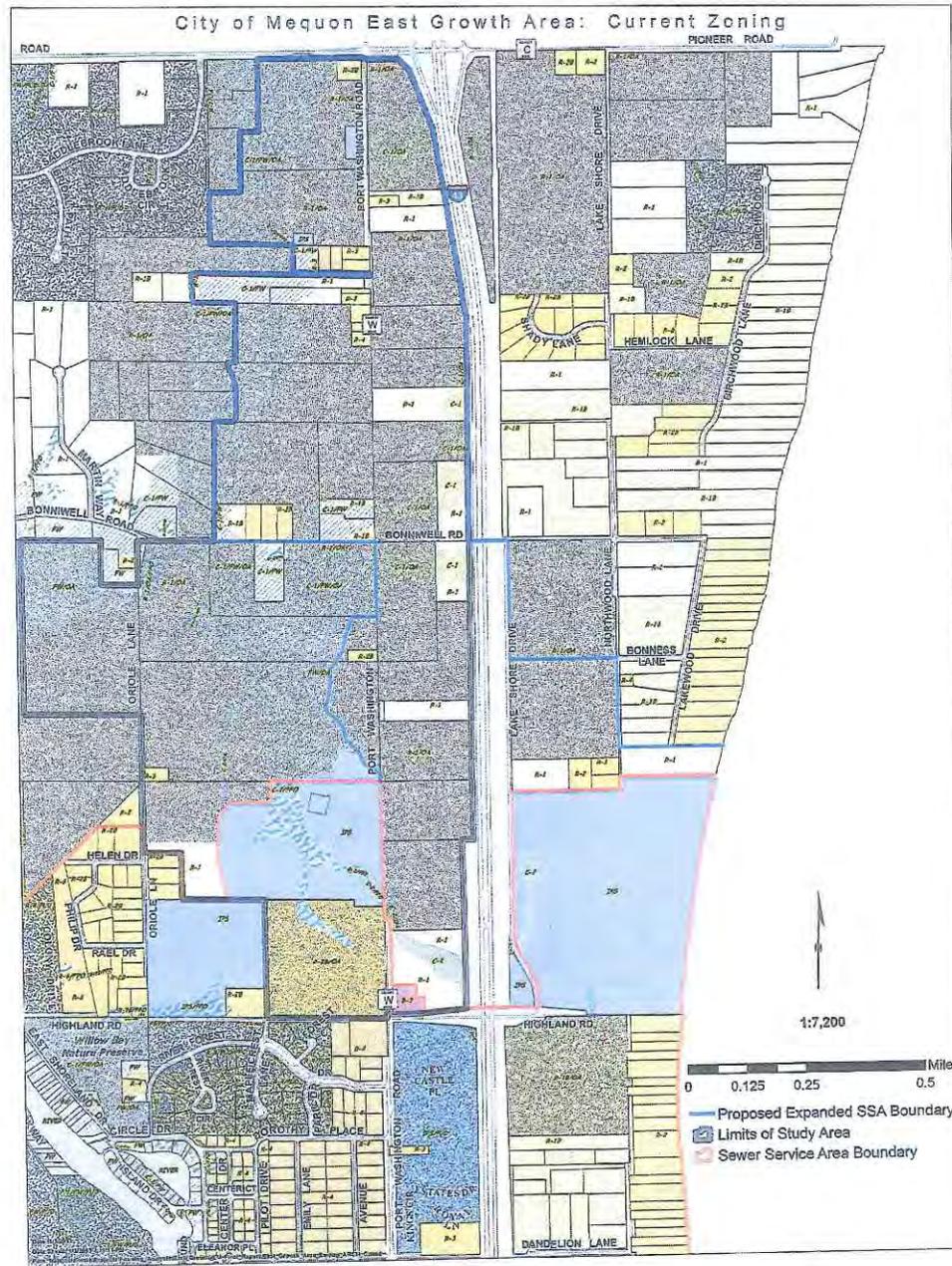
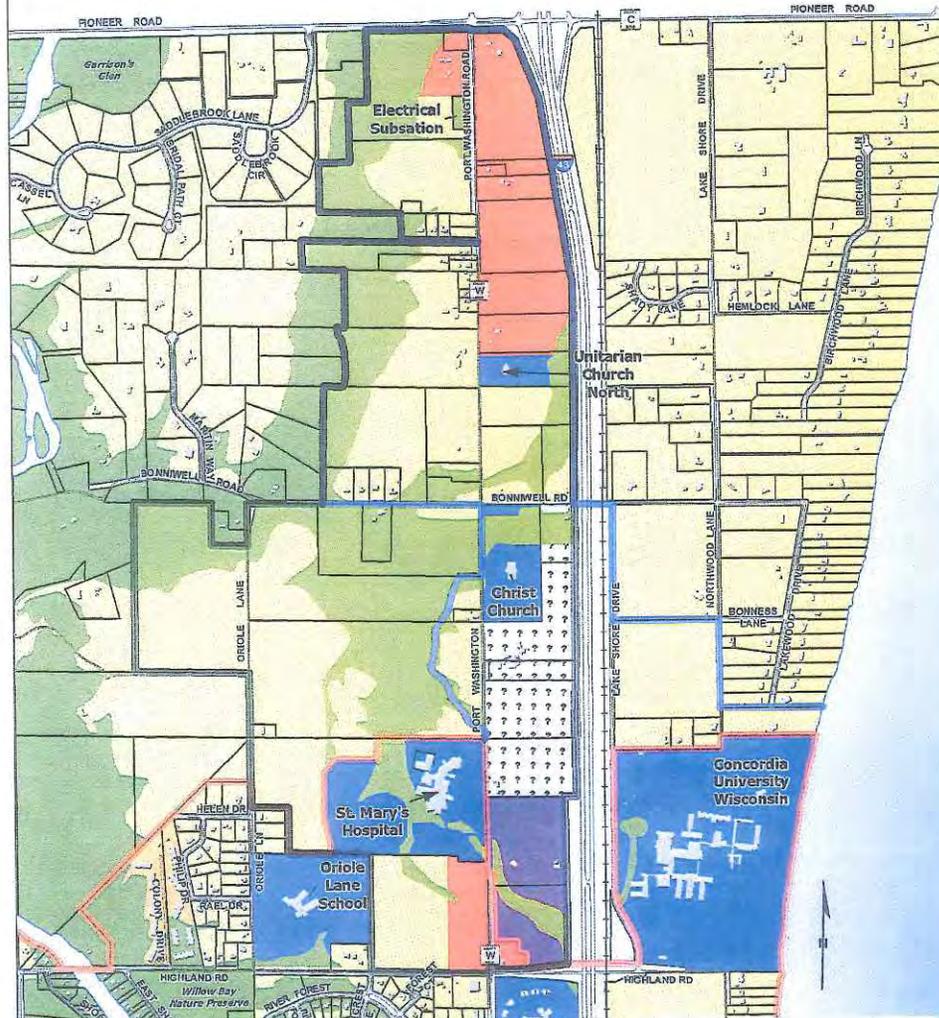


EXHIBIT E

City of Mequon East Growth Area: Highlanders Landuse Proposal



Team Highlanders Comments:

The following are the comments this team made while describing their reasoning for the land use choices made:

- Agreed with the boundaries of the study area.
- The group was split on the need for an interchange at Highland. If proposed, perhaps only as southbound. Suggested furthering utilizing the Pioneer Road's interchange.
- In favor of commercial at both intersection of Port/Pioneer and Port/Highland. Port/Highland intersection to include commercial recreational uses.
- The group was split on land uses for the east side of Port Washington Road. Some felt it was challenging for residential development given the railroad and I-43. If it were to develop as commercial uses it should be slated for low intensity type development such as headquarters and campus-like settings for single uses, not service oriented.
- The group did not achieve consensus on multiple family. There was concern expressed regarding the lack of owner-occupancy.
- On the west side of the corridor, the group suggests residential development with smaller lot sizes of 1 ac or less – more affordable, "tighter" residential development.

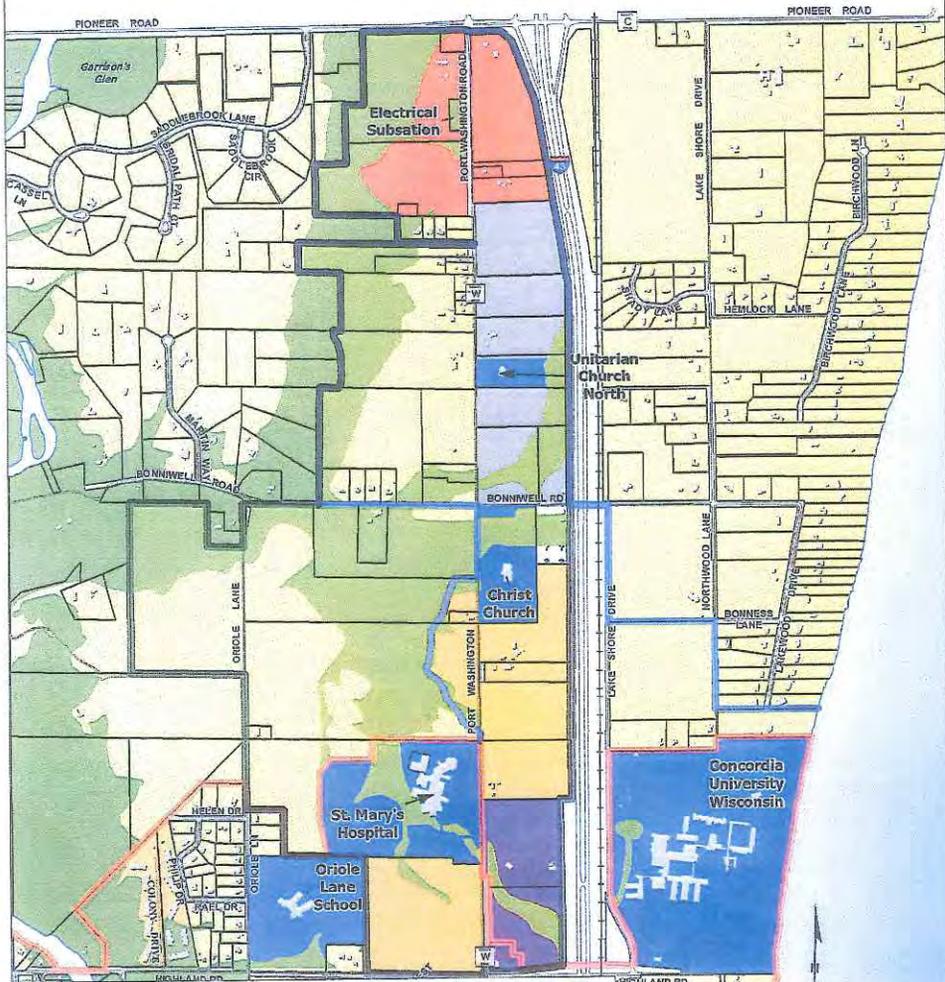
- Proposed Expanded SSA Boundary
- Limits of Study Area
- Sewer Service Area Boundary
- Proposed Changes to Existing Landuse**
- Highlanders Proposal**
- Undetermined
- Commercial
- Commercial/Recreational
- Critical Environmental
- Institutional
- Single-Family



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EXHIBIT E

City of Mequon East Growth Area: Orioles Landuse Proposal



Team Orioles Comments:

The following are the comments this team made while describing their reasoning for the land use choices made:

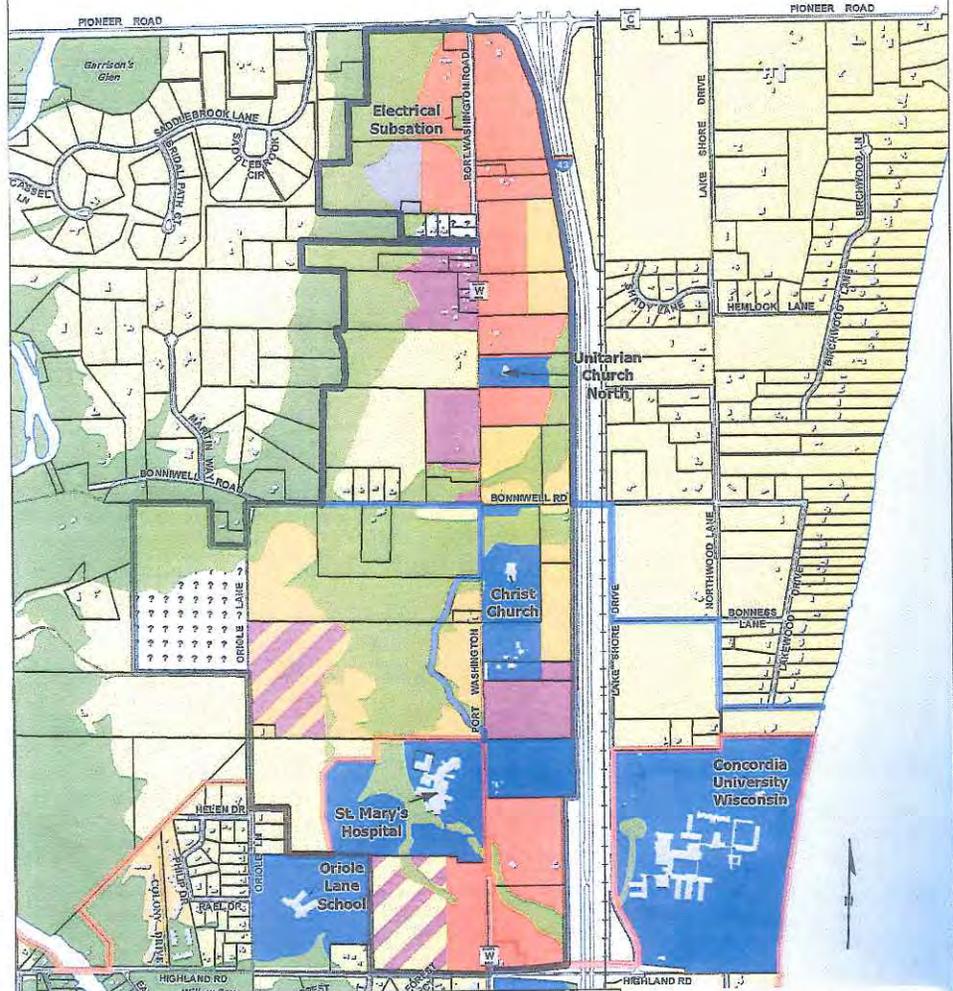
- Generally agreed with the boundaries of the study area except that the lots immediately west of Ulso Creek, immediately south of Pioneer should be considered.
- This group's land use decisions were based on two assumptions:
 1. There would be no interchange at Highland
 2. There would be public sewer and water extended to the entire corridor
 3. Utilize institutional uses as transitional sites.
- At Pioneer and Port Washington Road intersection allow more commercial development with industrial uses. Allow uses that rely on or leverage access to highway.
- Transitional uses from Pioneer Road to the south with office and neighborhood commercial uses.
- Add multiple family as a buffer to I 43 in and around Highland Road and St. Mary's.
- The group was split on the need for an interchange at Highland. If proposed, perhaps only as southbound. Suggested furthering utilizing the Pioneer Road's interchange.
- In favor of commercial Port/Highland intersection and include commercial recreational uses. The idea is to keep this commercial node at a more neighborhood service level.
- Allow residential development on Oriole Lane at 1.5 - 2 acre lot sizes.

Proposed Changes to Existing Landuse Oriole Proposal

- Undetermined
- Commercial
- Commercial/Recreational
- Critical Environmental
- Industrial
- Institutional
- Multi-Family
- Single-Family

— Proposed Expanded SSA Boundary
 Limits of Study Area
 Sewer Service Area Boundary

City of Mequon East Growth Area: Spartans Landuse Proposal



Team Spartans Comments:

The following are the comments this team made while describing their reasoning for the land use choices made:

- Generally agreed with the boundaries of the study area but should consider the property between I-43 and the RR Xing on the east side.
- In favor of an interchange at Highland. Commercial uses would be a natural fit at Highland if an interchange is constructed.
- Utilize the intersection of Port/Highland and Port/Pioneer as the commercial anchors to the corridor. Allow in order to create additional revenue. View these intersections to have advantages for commercial development given the larger size of the existing parcels.
- Port/Pioneer Road could support commercial and industrial uses.
- The area east of Port Washington Road doesn't lend itself to single family residential due to the traffic and noise of I-43.

- Proposed Expanded SSA Boundary
- Limits of Study Area
- Sewer Service Area Boundary
- Proposed Changes to Existing Landuse**
- Spartan Proposal**
- Undetermined
- Commercial
- Critical Environmental
- Duplex
- Duplex/Multi-Family
- Industrial
- Institutional
- Multi-Family
- Single-Family
- Single-Family/Duplex

EXHIBIT F

Open House Board Notes:

A map of the study area was available for the participants to leave anonymous notes. The following lists those comments.

- Data Center Industry
- Von Maur
- High End Department store
- Bank
- Pharmacy
- Restaurants
- Office
- Enhance natural waterways and incorporate into future developments by including a pathway system linking Highland to Pioneer Road.
- One acre residential possible in near future. Also working with the conservation fund to preserve Ulao Creek area.
- Entire Corridor Priorities:
 - 1) Zoning/Planning;
 - 2) Sewer and Water;
 - 3) Power;
 - 4) study I-43;
 - 5) study Port Washington Road

Duplexes: APPEALING

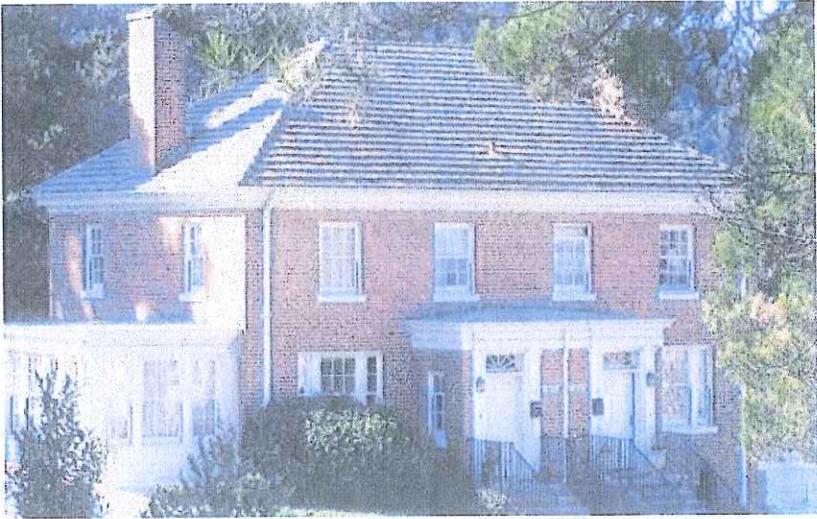


Exhibit G: IMAGERY RESULTS

Duplexes: APPEALING



Exhibit G: IMGERY RESULTS

Duplexes: MIXED OPINIONS



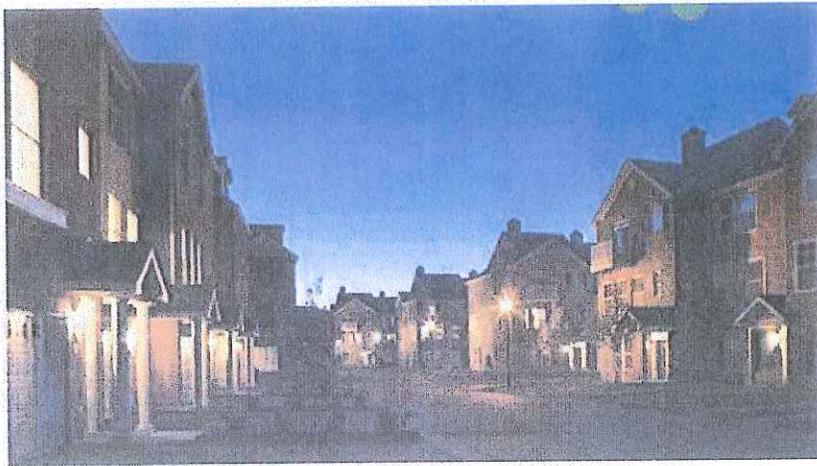
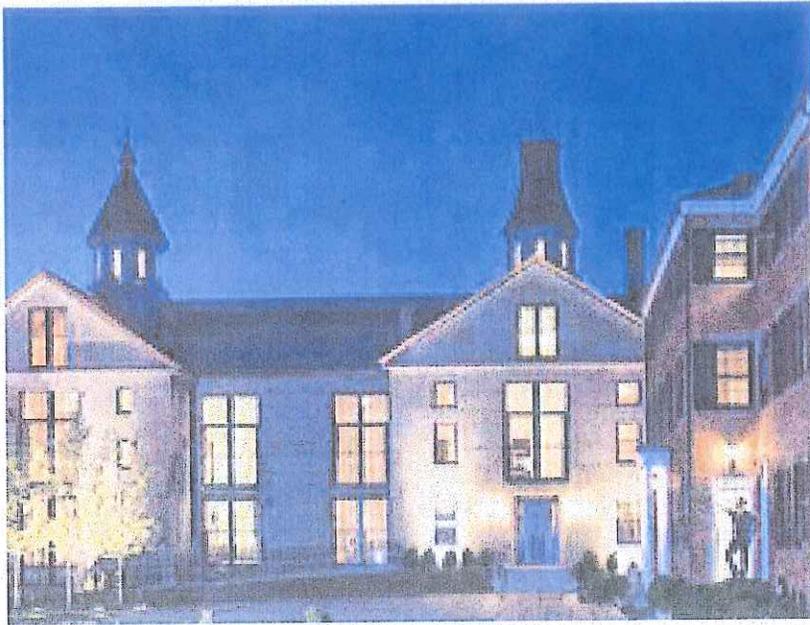
Exhibit G: IMAGERY RESULTS

Duplexes: DISPLEASING



Exhibit G: IMAGERY RESULTS

Residential Multi-family: APPEALING



Residential Multi-family: MIXED OPINIONS



Exhibit G: IMAGERY RE-

Residential Multi-family: MIXED OPINIONS

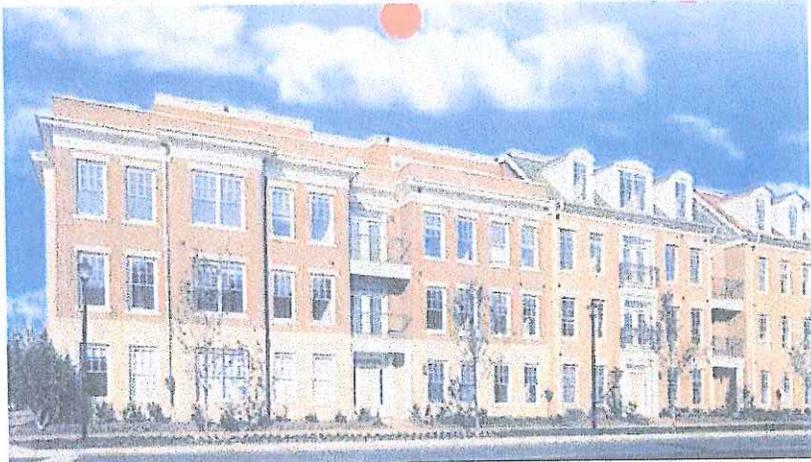


Exhibit G: IMAGERY RE-

Residential Multi-family: DISPLEASING



Exhibit G: IMAGERY RE-

Commercial 80,000sf: APPEALING



Exhibit I: IMAGERY RESULTS

Commercial 80,000sf: MIXED OPINIONS



NO OPINION

Exhibit I: IMAGERY RESULTS

Commercial 40,000sf: APPEALING

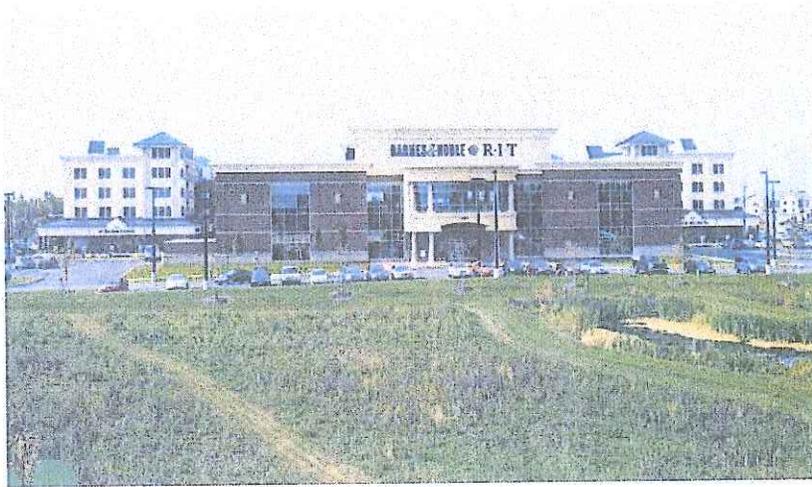


Exhibit G: IMAGERY RE-

Commercial 40,000sf: MIXED OPINIONS

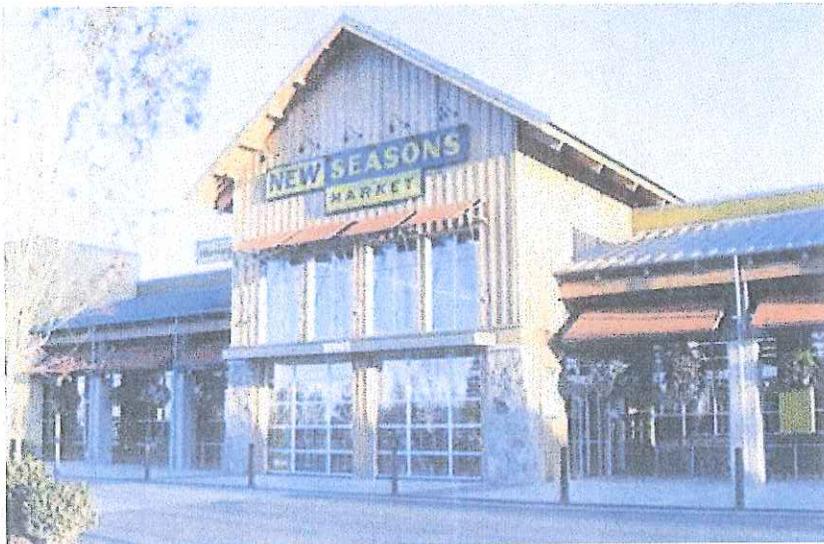


Exhibit G: IMAGERY RE-

Commercial 40,000sf: DISPLEASEING

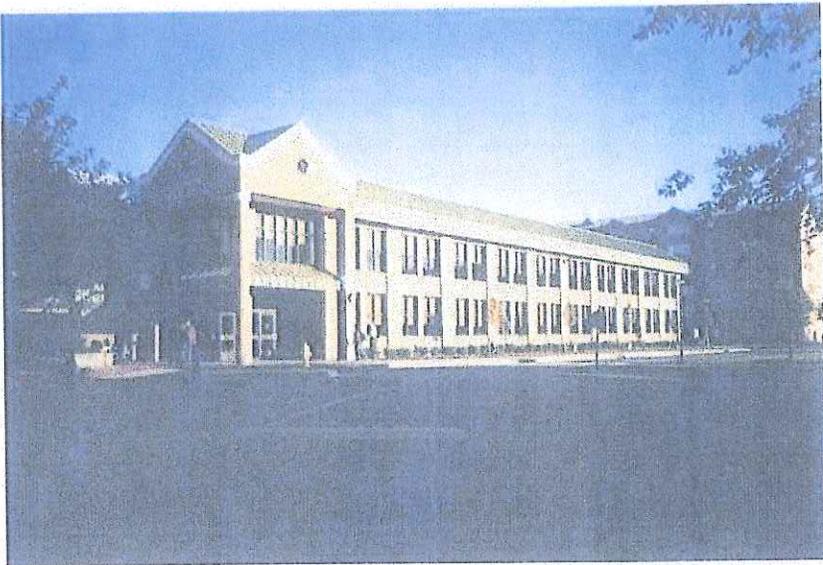


Exhibit G: IMAGERY RE-

Commercial 80,000sf: APPEALING



Exhibit G: IMAGERY RE-

Commercial 80,000sf: MIXED OPINIONS



NO OPINION

Exhibit G: IMAGERY RE-

Commercial 80,000sf: DISPLEASING

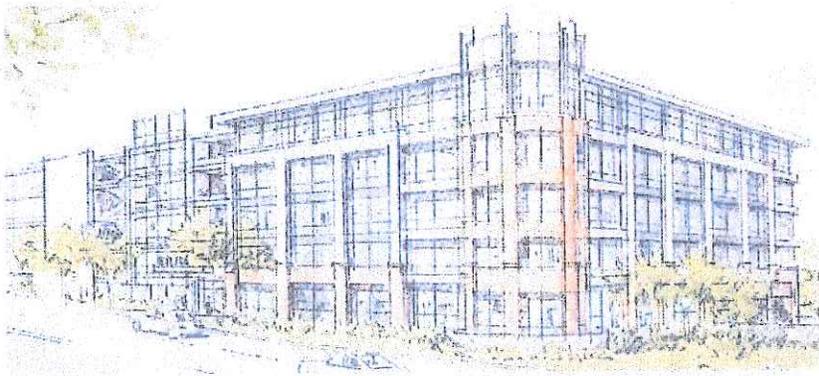


Exhibit G: IMAGERY RE-

Commercial +100,000sf: APPEALING

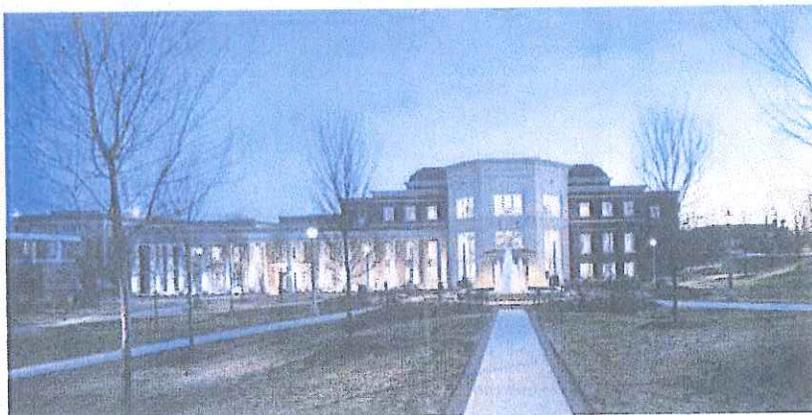
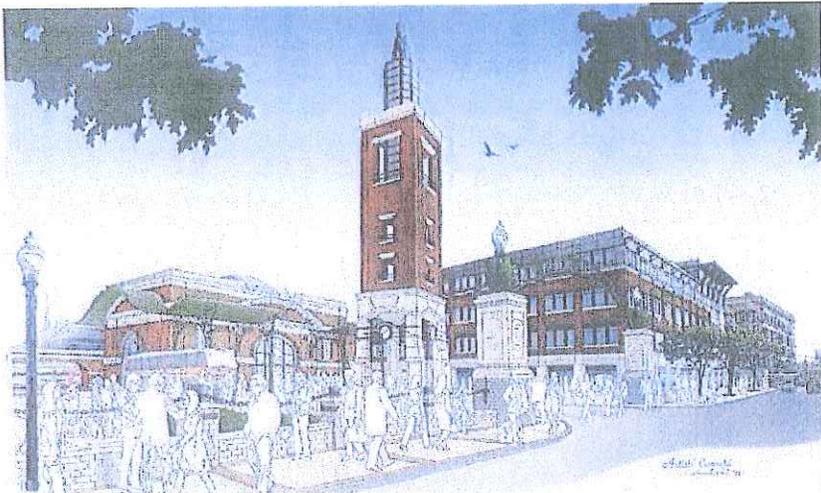
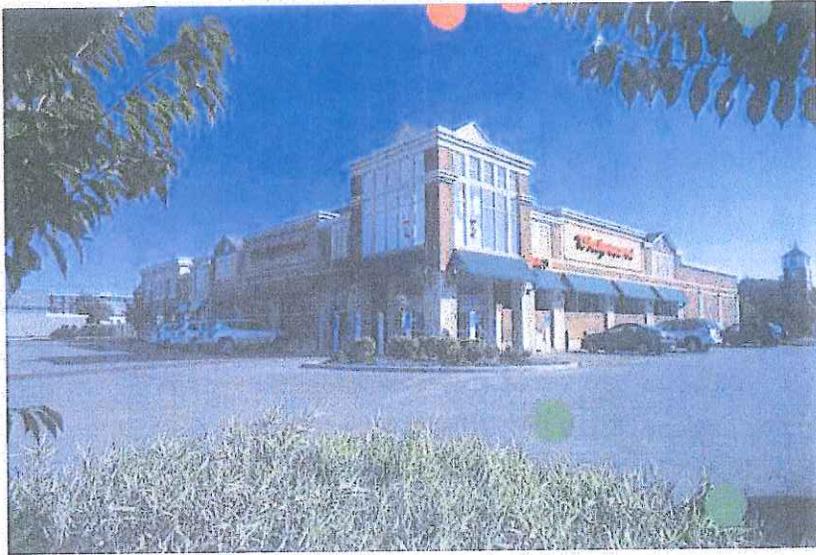


Exhibit G: IMAGERY RE-

Commercial +100,000sf: MIXED OPINIONS



NO OPINION

Exhibit G: IMAGERY RE-

Commercial +100,000sf: DISPLEASING



Exhibit G: IMAGERY RE-

ROADWAY DESIGNS : APPEALING

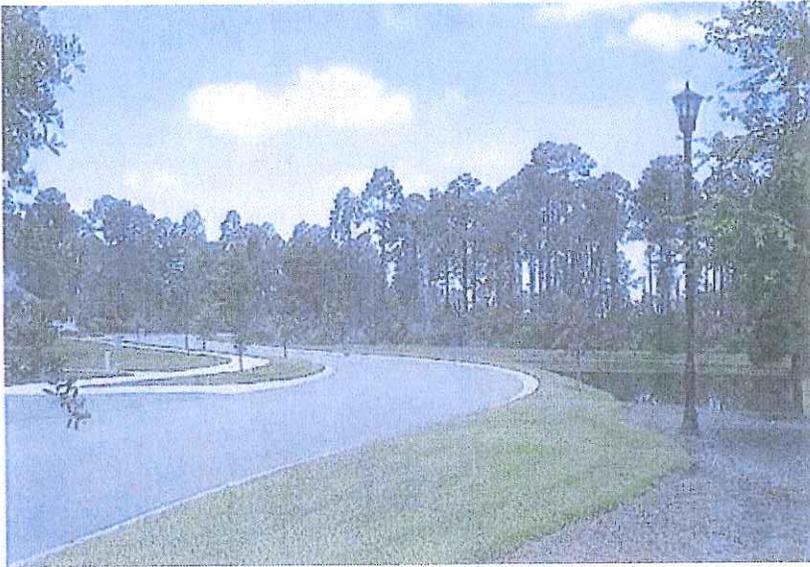


Exhibit G: IMAGERY RE-

ROADWAY DESIGNS: DISPLEASES



Exhibit G: IMAGERY RE-

ROADWAY DESIGNS: DISPLEASEING

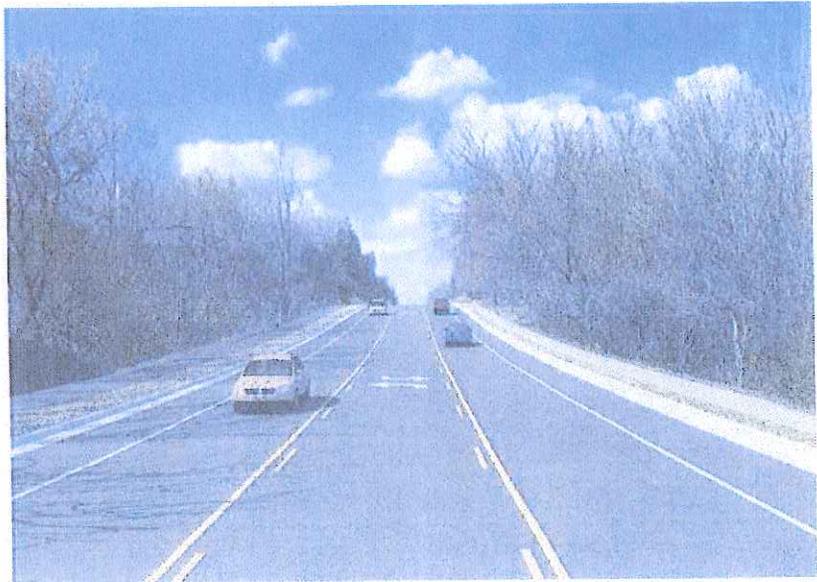


Exhibit G: IMAGERY RE-

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Department of Community Development

NEIGHBORHOOD PLAN OPEN HOUSE East Growth Area Land Use Committee August 22, 2012 6:30pm – 9:30pm MINUTES

1. Call to Order, Roll Call.

Members Present: Ald. Mark Seider, Alt. Ald. Dan Abendroth, John Stoker,
Dave Romoser, Tim Carr, Alt. Bill Arpe, Fred Bersch, Carl
Vogel, Michelle Ziegler

Staff Present: Kim Tollefson, Director of Community Development

2. Welcome and Introduction by Committee Chair, Alderman Seider

Alderman Seider explained the purpose and intent of the East Growth Area Land Use Committee. Committee members followed by introducing themselves.

3. Study Area and Conditions: presentation by Director Tollefson

Kim Tollefson, Director of Community Development presented a power point to group members and property owners from the area.

Ms. Tollefson noted there were originally three growth areas that Council focused on for economic and housing development initiatives. She continued that housing was determined to be the focus of growth in two of the three areas in the community. The east growth area could present opportunities for a variety of land uses for three reasons: 1) adjacent to sewer/water utility in existence already, 2) close to activity centers, and 3) easy access to commuter routes, job centers and schools.

Ms. Tollefson stated that the Sewer Utility Commission further directed staff to look at this corridor because of expanding needs from Concordia University. Staff began by identifying the boundary for the study area. Questions that were asked initially were:

- should the area remain rural residential 5 acre zoning
- what is this the highest and best use of the land.

She noted that development of the area had been stagnant for some time.

Ms. Tollefson pointed out that there are challenges in keeping the commercial corridor vital because retailers and business have density requirements surrounding their businesses. She stated that additional housing units in the east growth area could help with vitality in the commercial corridor to the south. She continued that Common Council had a discussion about the proper amount of additional dwelling units to be added (other than single family) and that the 2035 Comp Plan suggests 30%.

Ms. Tollefson summarized the goal of the group and what was to come. She gave directions for the mapping exercise. One property owner than questioned what if they do not want growth to occur in the area. Ms. Tollefson replied that for this exercise the group needed to assume growth was going to occur.

4. Mapping Exercise for Property Owners

The property owners were split into five different groups and asked to complete a mapping exercise in which each group would report on after approximately 45 minutes time. (See August 22nd, 2012 East Growth Area Committee Packet for group mapping instructions.)

Group Summaries

Each group presented summaries of their proposed land use concepts.

Wildcats: This team had questions regarding what areas of wetlands were not usable for development purposes. Most of the group was in favor of an interchange at Highland Road but had questions / concerns regarding what it would look like. Mr. Epperson, owner of Missing Links Golf Course questioned if it would destroy any portion of his golf course. They developed the land for this exercise in order to help Mequon generate income, and most group members felt there was not enough affordable housing within Mequon so they included a multiple family area. The Wildcats placed commercial uses on both ends of the district with housing in the middle. They also included industrial close to the entrance to the highway.

Spartans: This team acknowledged that this is a major thoroughfare going north and south which generates a lot of traffic and noise and does not lend itself to residential single family but rather multiple family (RM). They included Commercial, RM, industrial and green space. Mark Maletske suggested looking at how Grafton setup their exit and how they have anchor tenants that fund the infrastructure surrounding a freeway exit.

Chinooks: This team felt that commercial zoning was appropriate for this area at both ends. They suggested that a gateway feature must be attractive. The Chinooks felt that with wetland areas in the middle of the area it might be a good area for multi-family. Mr. Al Hospel (not a member of the Chinook's team) gave an explanation of how WE Energies has requested to make use of his land to expand a substation there.

Highlanders: This team placed commercial uses near the freeway interchange. On the west side of Port Washington Road they felt would be a good use for conventional residential neighborhood with lots of 1 acre or less. The Highlanders decided that the noise from the freeway would make the east side of Port Washington Road suitable for commercial / light industrial uses, not to include services, but possibly including a company headquarters. The group was opposed to RM because of the idea of rentals. They were also split on the interchange at Highland Road and thought maybe it could have an entrance / exit going just one direction.

Orioles: This team favored not having an I-43 interchange. They placed higher density single family residences on the western side of Port Washington Road and felt the multi-family residential would make a good buffer to the freeway. They also placed light industrial and commercial along the east side of Port Washington Road. They suggested 1.5 to 2 dwelling units per acre in the middle section of the corridor and to include duplexes there. Michele Ziegler stated that Ula Creek always floods and she would like to see walking paths and enhancement if residential was placed near it. Dave Romoser questioned the DNR's motivation for buying property in the area. Ms. Tollefson replied that they are asking to construct a small parking lot. A property owner in the area stated that a lot of people hunt

land there. Mr. Romoser suggested that an argument could be made that three to four story buildings on the east side of Port Washington Road provide a sound barrier from the freeway. Mr. Maletzke pointed out that the freeway be creating in the 1950s created man-made wetlands.

5. Summary and Announcements

Ms. Tollefson stated that at the September Common Council meeting she will present an overview from this meeting and what the next steps are in the process. There was some discussion as to a height limit on the east side of Mequon Road. Ms. Tollefson noted that in 2008 the Fixes applied to changed zoning of their parcel to Rural zoning and the application was tabled and has yet to be called back up.

Mr. Romoser questioned if auto dealerships would be a fitting use for any portion of the area. He noted that high end auto dealers look for visible and accessible land and are good sources of revenue. Mr. Carr stated that a while ago International Autos thought about coming to Mequon but was voted down. Mr. Romoser stated that Honda built a dealership in Kenosha because of its highly visible location.

Lynn, a representative of Concordia, questioned if restaurants would be a good addition to the area. Mr. Romoser stated that for a lot of restaurants Mequon does not pass the density filter. One homeowner questioned if we want to create commercial that serves Mequon citizens at a local level or draws people in from a larger surrounding base and serves regional needs. Ms. Tollefson stated that policy has been to serve Mequon citizens and not create a draw. An outlet mall was questioned as a possibility because it fills that desire for specialty boutique type stores and might be positioned well between these facilities and Pleasant Prairie, Oshkosh and Johnsons Creek . A citizen stated that the Marcus theatre was a good example of where Mequon went outside the box and allowed something with a regional draw and it is a good example of where everyone can win.

6. Adjourn

EXHIBIT I

City of Mequon East Growth Area: Common Council Concept A

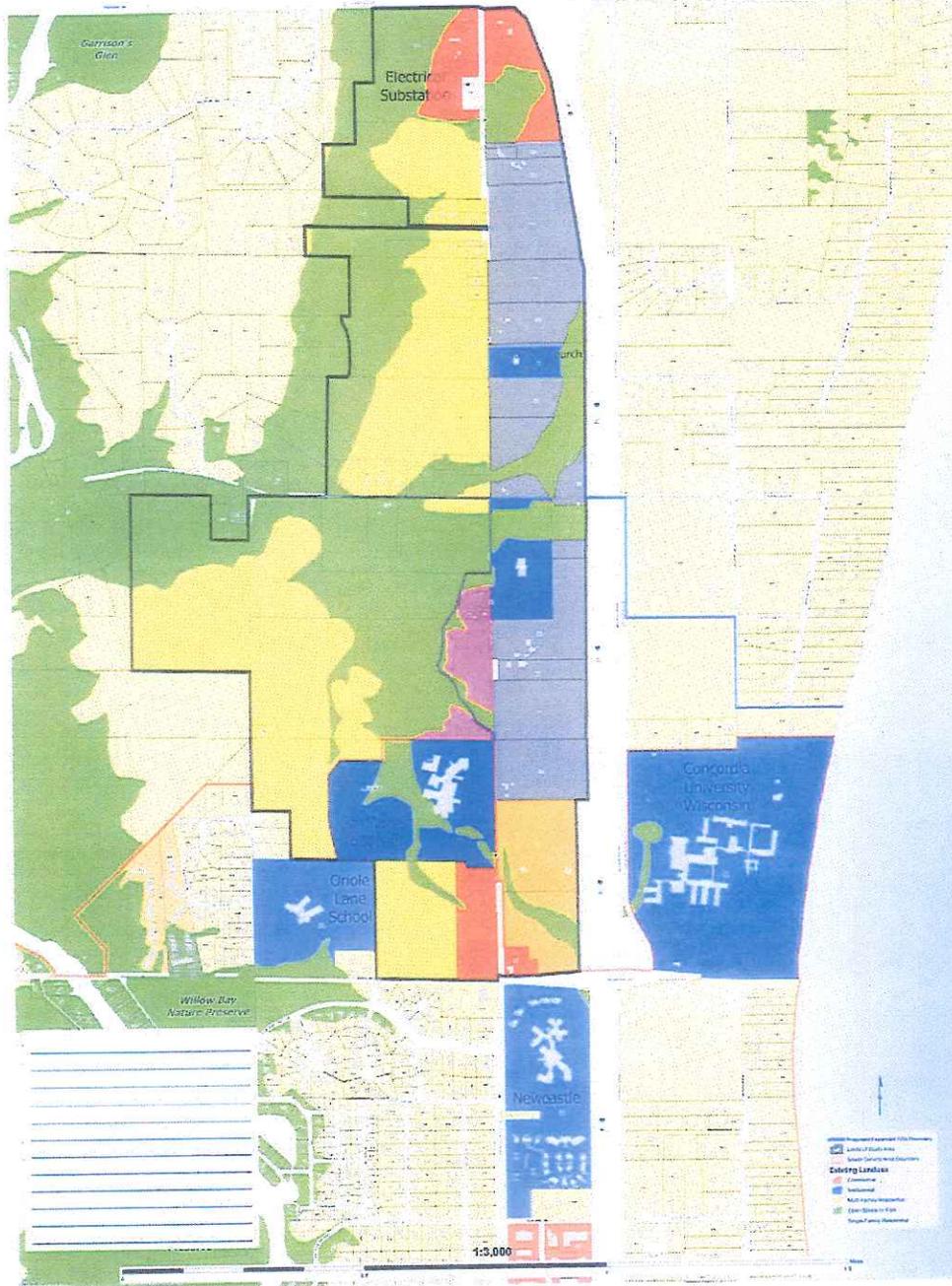
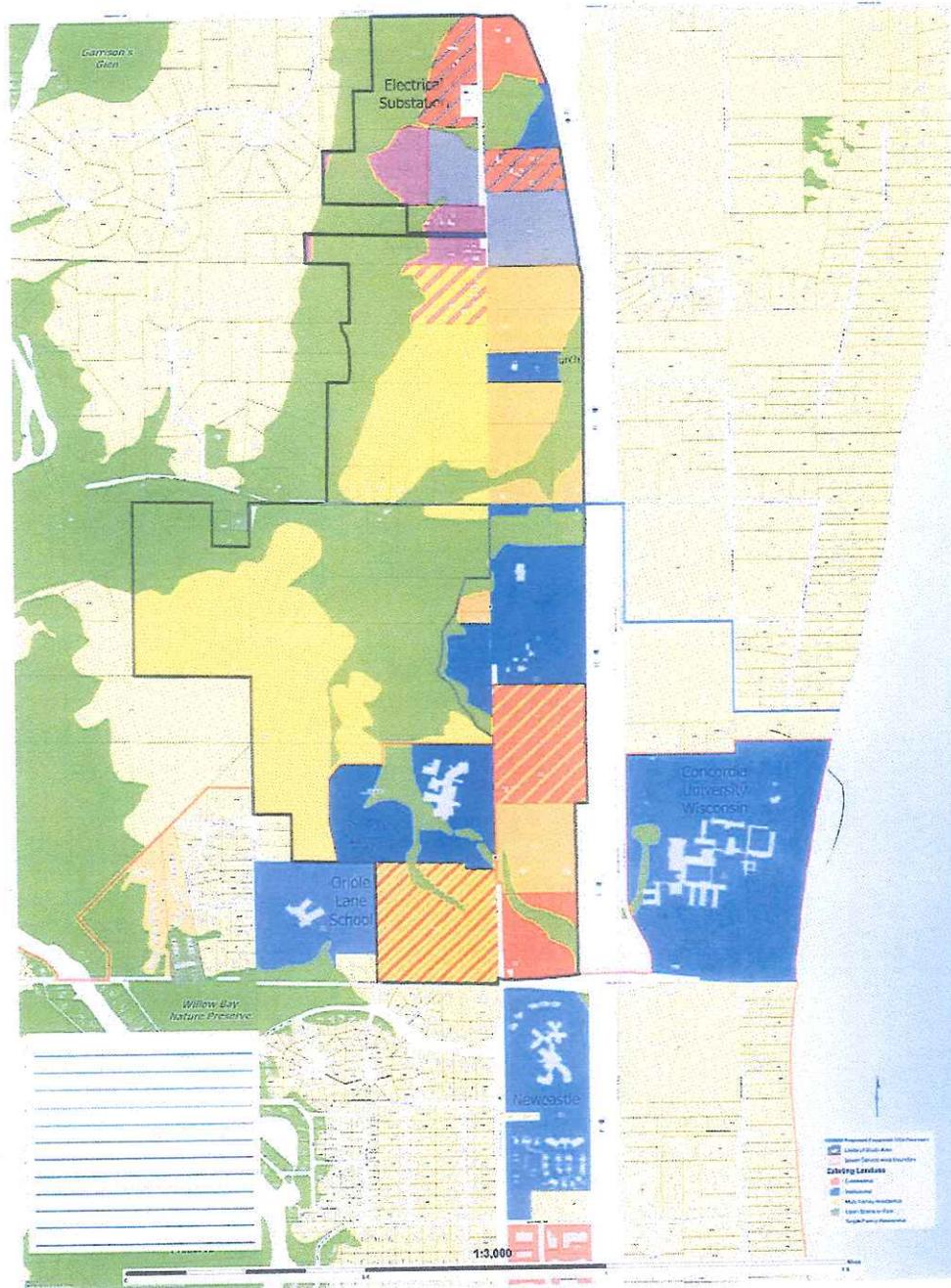
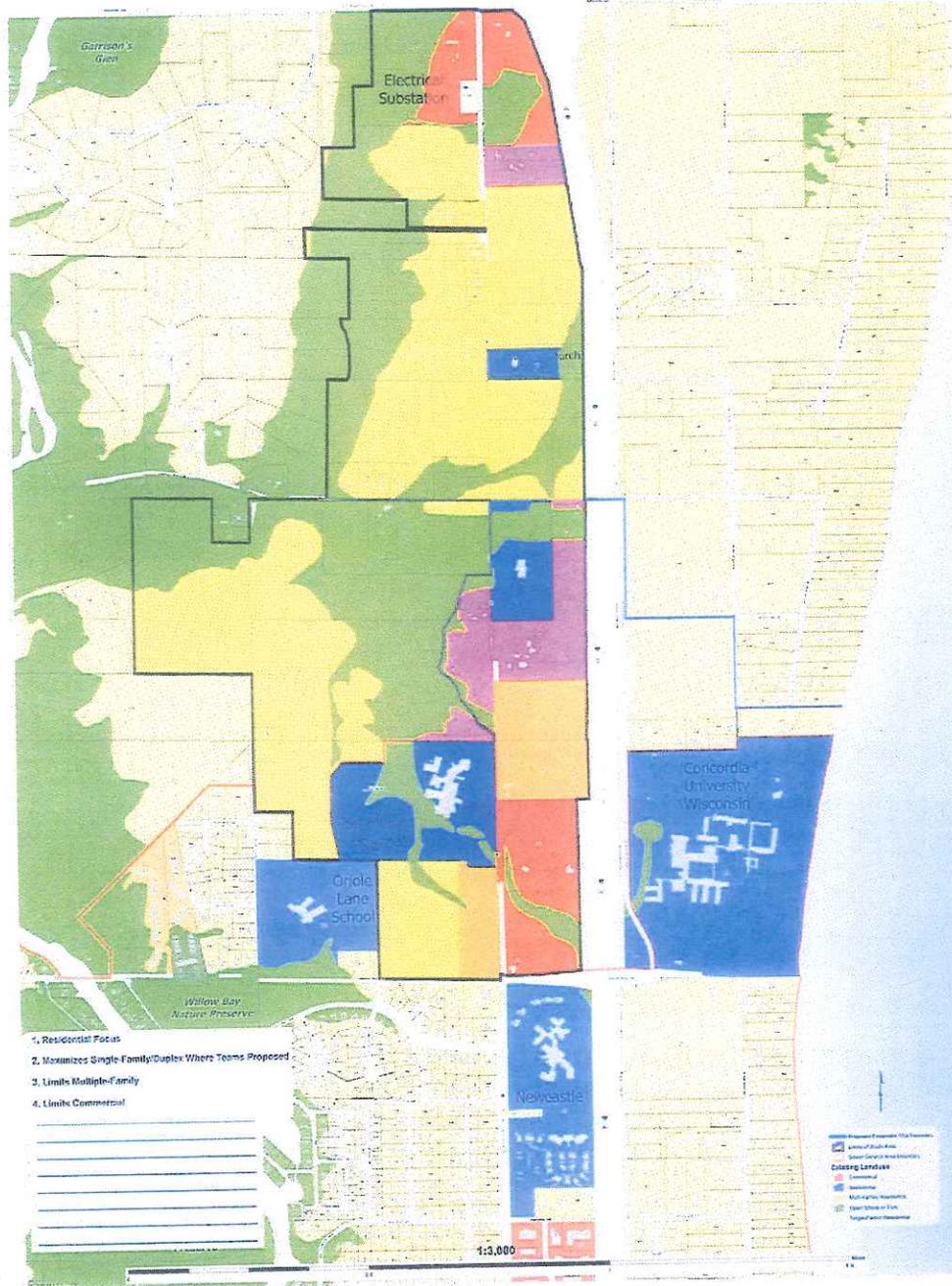


EXHIBIT I

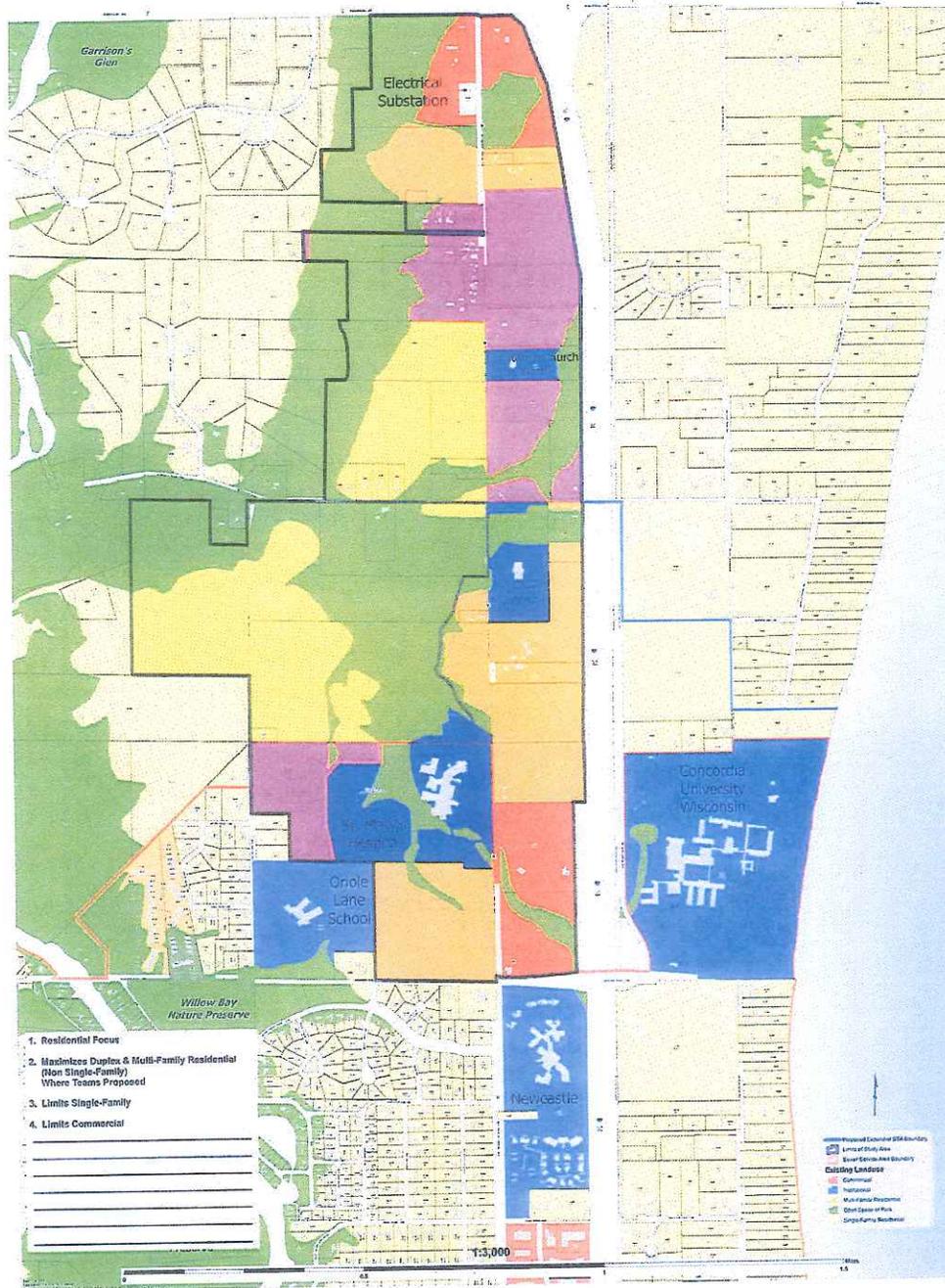
City of Mequon East Growth Area: Common Council Concept B



City of Mequon East Growth Area: Residential Land Use Focus



City of Mequon East Growth Area: Residential Land Use Focus



City of Mequon East Growth Area: Commercial Land Use Focus

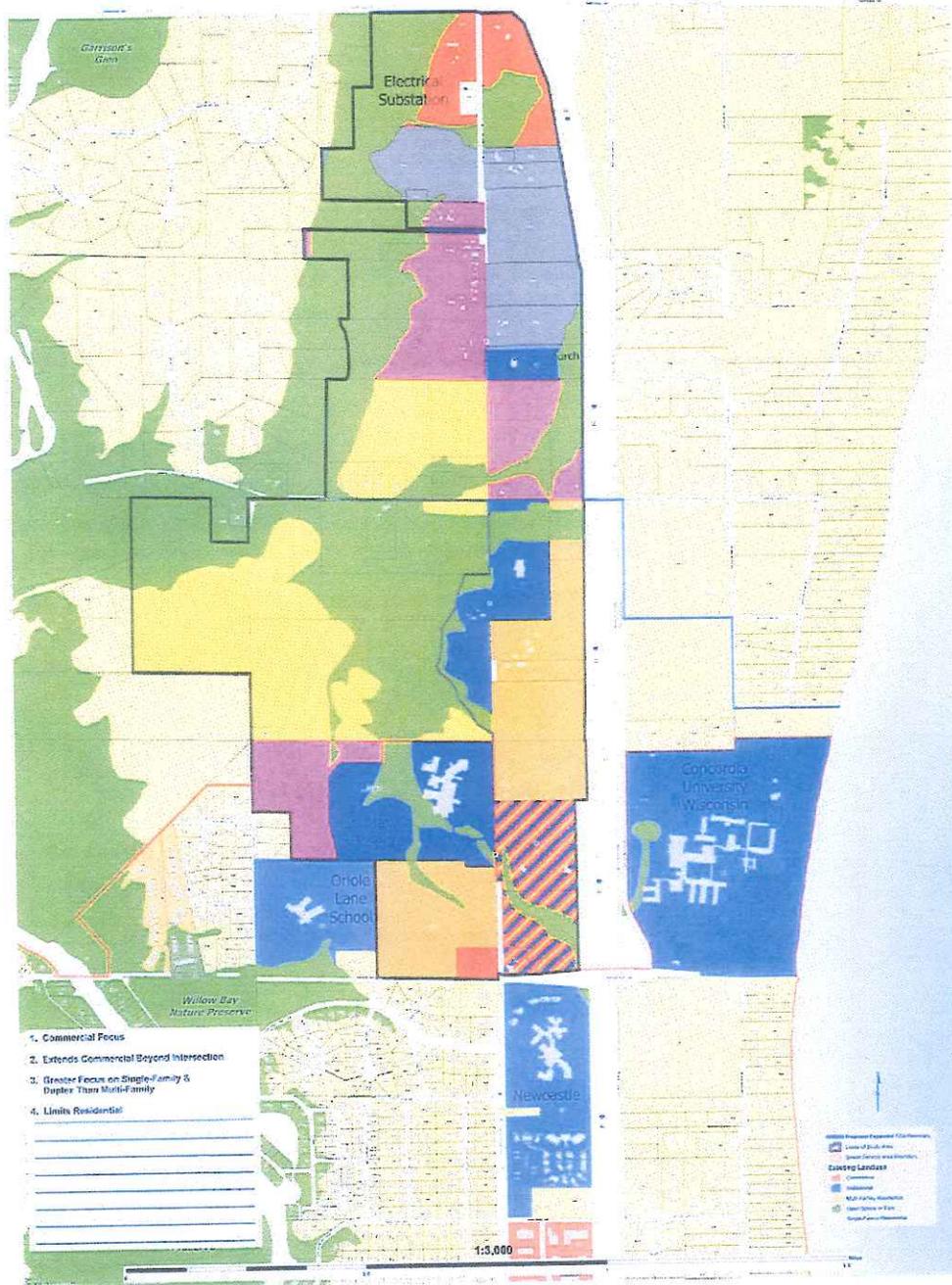


EXHIBIT L

