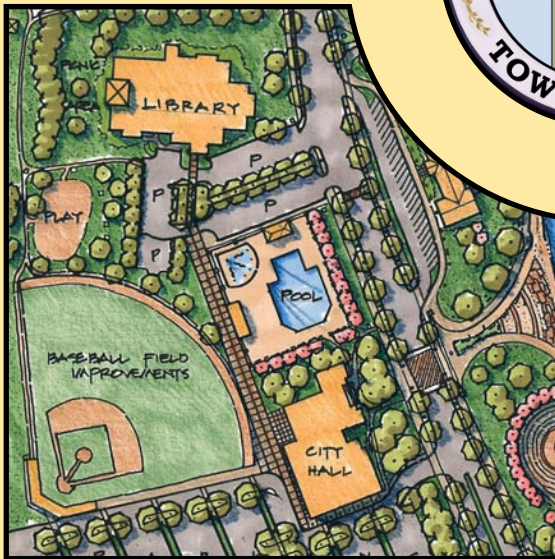
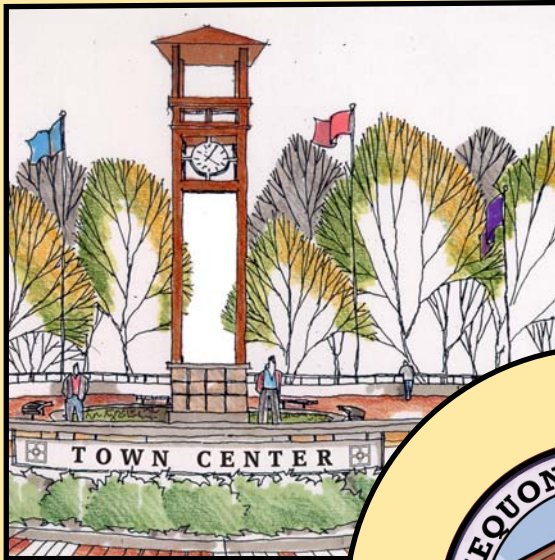


Mequon - Thiensville Town Center

Design Guidelines and Phase I Concept Design Plans

December 2005



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Preface: Introduction

The Town Center Phase I Concept Design Plans and Design Guidelines work together to establish a framework for development in the Mequon/Thiensville Town Center. Both the concept plans and design guidelines build upon the Town Center Master Plan done by Teska Associates. The concept plans deals with development by the public sector, such as streetscape, roadway, civic campus and park improvements. The design guidelines address the private sector issues, such as setbacks, building facades and materials. The concept plans and design guidelines seek to coordinate the development efforts between the public and private sectors to produce a unique sense of place for the Mequon/Thiensville Town Center.

1. Study Area

The Town Center study area encompasses the main north/south and east/west road corridors in the Mequon/Thiensville core. The north/south corridor follows Main Street/Cedarburg Road from Mequon Road to Concord Place and also includes the Green Bay Road corridor from Buntrock Avenue to Freistadt Road. The Town Center also includes the Mequon Road corridor from Buntrock Avenue to the Milwaukee River.

2. Design Districts

A shared set of design districts provides organization for the concept plans and the design guidelines. While these districts have their own unique character and function, a common theme and design elements link the districts to form the Town Center. The following design districts have been established:

- A. The Historic Village
- B. Expanded Village Along Main Street
- C. Arrival Corridors
- D. The Civic Campus

3. Town Center Goal & Objectives

The main goal of the concept design plans is to strengthen the Town Center as the heart of the two communities. The Town Center can be strengthened by building upon the attributes of the existing area to create a unique character



Improvements to Date:

- Rotary Riverwalk - Phase I and II
- Wayfinding Signage
- Inner-Urban Bike Trail

that has safe walkable streets, human-scale architecture, public spaces and common design features to establish a sense of place for the Town Center. A strong sense of place makes the Town Center an attractive location for shopping, working, visiting and living, thereby, increasing the likelihood for an economically viable commercial business district. The following is a list of objectives to achieve this goal.

- Create the Town Center as one place, with distinct design districts that take advantage of the unique characters within the area.
- Preserve and complement the historic village character.
- Improve the mix of uses within the Town Center.
- Provide visual and physical access to the Milwaukee River.
- Design for "walkable" streets, while maintaining functionality for all traffic types.
- Promote interconnectivity within the Town Center.
- Coordinate public streetscape improvements with private improvements.
- Create gateway features at the entranceways into the Town Center.
- Improve the aesthetics and recreational aspects of the Mequon Civic Campus.
- Encourage private property owners to make improvements that meet a high standard of design and maintenance.
- Make streetscape improvements to enhance the aesthetic character of the area.

4. Organization of the Report

The report is organized into the following chapters:

Preface	Introduction
Chapter 1	Design Guidelines
Chapter 2	Concept Design Plans - Phase I



Chapter 1: Design Guidelines for Private Property

1. Introduction & Overview

The following design guidelines have been prepared for the Mequon/Thiensville “Town Center” area. The Town Center area is a large study area comprised of several unique parts. The guidelines in this report provide general recommendations for the entire Town Center in addition to more detailed guidelines that are specific to certain parts of the study area.

1.1 Chapter Organization

The report is organized into the following sections:

1. Introduction & Overview
2. Design Guidelines for All Districts
3. Design Districts
4. Specific Guidelines for the Historic Village
5. Specific Guidelines for the Expanded Village at Main Street.
6. Specific Guidelines for the Arrival Corridors.

1.2 Relationship to Other Guidelines and Ordinances

Several guidelines and ordinances shape the physical form of development in Mequon and Thiensville. Thiensville, for example, has adopted detailed architectural guidelines for the center of its downtown, referred to in this report as “The Historic Village”. These guidelines are adopted by resolution and enforced through the Plan Commission and the Historic Preservation Commission. Although Mequon does not currently have specific design overlay districts, the City does review site and architectural plans. Additionally, Mequon and Thiensville both enforce a signage ordinance.



The following guidelines are intended to supplement these existing standards. The existing standards have served both communities for some time.

The new guidelines offered in this report will provide consistency between the communities and provide a design framework for new opportunities that have been explored throughout the Town Center planning process.

These guidelines represent the recommendations for the Village and City to share with property owners and the development community. The City and the Village will have the opportunity to formalize the enforcement of these recommendations through the adoption of overlay districts or other form-based codes.



1.3 Design Guidelines Overview

The following guidelines include recommendations for site and architectural improvements. They have been prepared for the "Town Center" portion of Mequon and Thiensville.

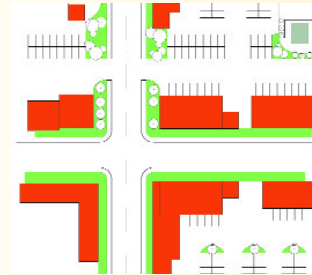
The purpose of the guidelines include:

- Preserve and complement historic character
- Promote unique and creative design solutions
- Promote coordinated design decisions between the communities
- Coordinate improvements between private property and public spaces

The Town Center has been divided into three design districts. General guidelines have been created for the entire Town Center and specific guidelines for each district.

General Site Design Guidelines

- Bring buildings close to the sidewalk.
- Locate buildings parallel to the street.
- Move parking to sides and rears of buildings.
- Screen parking lots from the public view.
- Promote shared parking and drives between buildings.



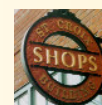
General Building Guidelines



- Encourage 2-3 Story buildings, utilize parapet walls and gables for height.
- Design all visible sides of the building.
- Utilize brick and native stone as primary materials on visible sides of buildings.
- Encourage varied roof forms- incorporate gables to reflect local character.
- Avoid blank building walls.
- Create variations in roof and façade lines to promote visual interest.
- Divide larger buildings into segments that resemble smaller buildings.
- Require franchises to meet guidelines and local architectural character..
- Restore - Do not mask original architectural details.
- Create "open", transparent storefronts.

General Sign Guidelines

- Enforce existing signage ordinances.
- Signs should never mask architectural details.
- Limit back-lit signs and encourage lighting from external lighting sources.
- Create signs that are integrated with building's architecture.



2. Design Guidelines for All Districts

The following guidelines apply to all buildings and sites in the study area. These guidelines provide generalized recommendations to help promote coordinated and sensitive design decisions throughout the study area. It is important to note that there is no "one-size-fits-all" design solution. These guidelines recognize the great variation that occurs throughout the area. Specific guidelines pertaining to individual areas of the district are found in Sections 4 through 6.

2.1 General Building Design Guidelines

The design of buildings is an important part of shaping the character of the entire Town Center area. New buildings and the renovation of existing buildings should enliven the public streetscape, complement the character of adjacent buildings, and provide inviting entrances to pedestrians.

1. For new construction, encourage distinctive design that complement existing historic buildings.
2. Create buildings with details and proportions that are scaled to the pedestrian.
3. Building height adds character to the street. Consider two, three, and one story buildings with extended facades.
4. Complement the scale and character of adjacent and historic buildings.
5. Encourage mixed-use buildings.
6. Create inviting entrances to buildings.
7. Design all sides of the buildings that are visible from public streets and spaces.
8. Encourage franchises to provide building and site designs that match the overall historic design character through building placement, the use of materials, rooflines, signage, and proportions.
9. Incorporate features such as gabled roofs, dormers, and other features required in the historic district, through out the entire study area.



Architectural Detail provides scale to buildings.

Details such as windows, porches, varied materials help make buildings attractive to pedestrians.



General Building Composition. Regardless of style, buildings should have a distinct base (how the building meets the ground), middle, and top (how the building meets the sky).



2.2 Architectural Style and Character

No one architectural style is required for the Town Center Area. Architectural style and character should be based upon the suitability of a building for its purposes and legibility of the building's use. Additional considerations include the building's placement, proposed massing, proportion, scale, orientation to public spaces, orientation to other buildings, use of materials, and other attributes that impact the character of the proposed development.

However, buildings should be compatible with each other and share a common “architectural language”. This includes incorporating common design elements such as varied roof forms, minimized setbacks, scaling, massing, materials and architectural detailing as provided herein.

It is not the intent of this section, nor of these guidelines, to encourage duplication and repetition of existing structures. Rather these guidelines provide a framework to allow for inventive solutions and distinctive buildings that will not detract from existing buildings or the historic character of the district.

2.3 Building Materials

Materials add variety and richness to a building. The following is a list of building materials that are acceptable for use in all areas and districts within the Town Center. This material list is based, in part, on the material guidelines contained in the Thiensville Architectural Guidelines. These existing standards have been adapted to meet the varied needs of the study area as well as to provide additional guidance and direction to meet the stated design principles. As noted in the adopted guidelines, some materials may have specific restrictions which are described per individual material. It is important that buildings are reviewed on a case by case basis to insure conformity with the guidelines and their intent.

Acceptable Primary Materials

Primary materials are those that make up at least 80% of the solid (non-window) portion of any elevation. Acceptable primary materials include the following:

- **Common size brick** is acceptable for the construction of all building types. Special sizes and shapes are acceptable only as accents and decorations.
- **Native Stone** including limestone, fieldstone or lannon stone is acceptable on any building type.
- **Pre-Cast Stone / Pre - Cast Concrete** As primary materials, these would be allowed on a conditional basis requiring City/Village staff recommendation and approval by committees. These materials have a wide range of finishes, many of which could be appropriate in the Town Center.
- **Other Materials:** Other materials may be allowed specifically in each development district. These supplemental standards are described in Sections 4 through 6.

Acceptable Accent and Secondary Materials

Accents and secondary materials comprise less than 20% of the solid portion of any elevation. These materials include:

- **Precast Concrete and Cast Stone** are acceptable when used as accents, lintels, sills or decorations. Other uses shall be considered on a case by case basis.
- **Terra Cotta** is an acceptable material as cladding or accent.
- **Stucco** is acceptable as a wall finish material on upper floors and gables.
- **Wood Siding** is acceptable on upper floors and gable ends or as roofing materials.

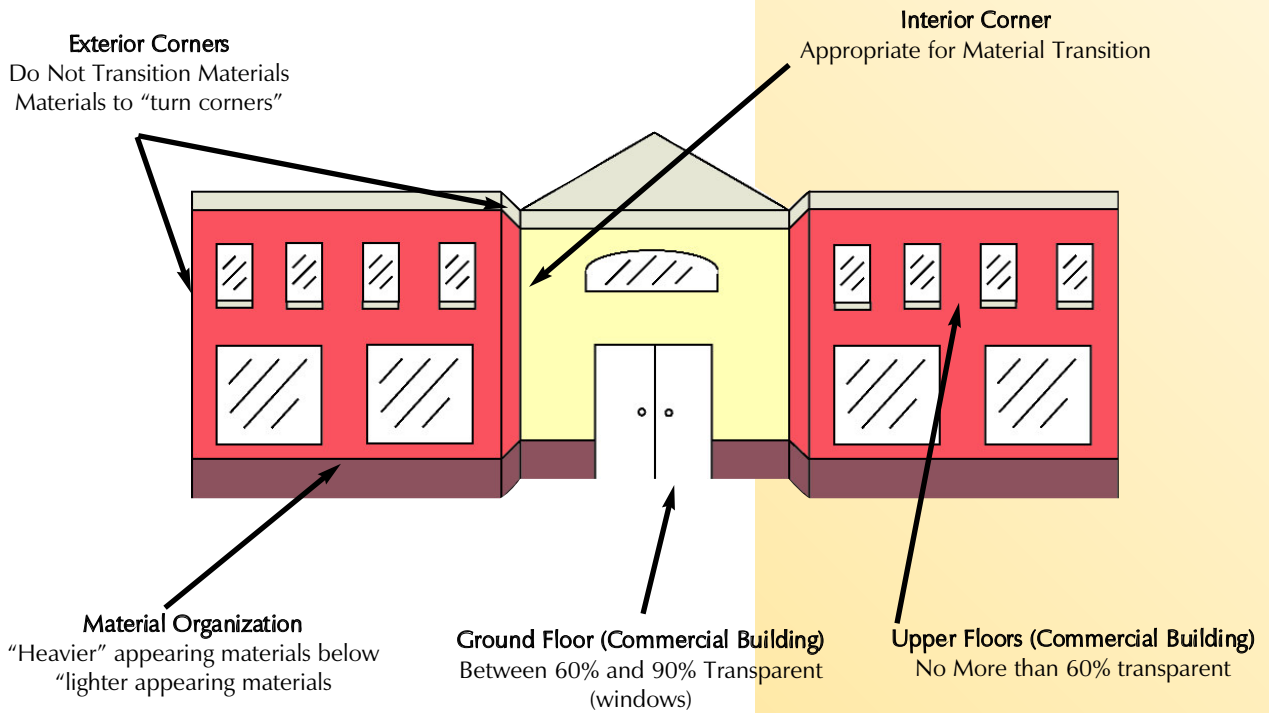
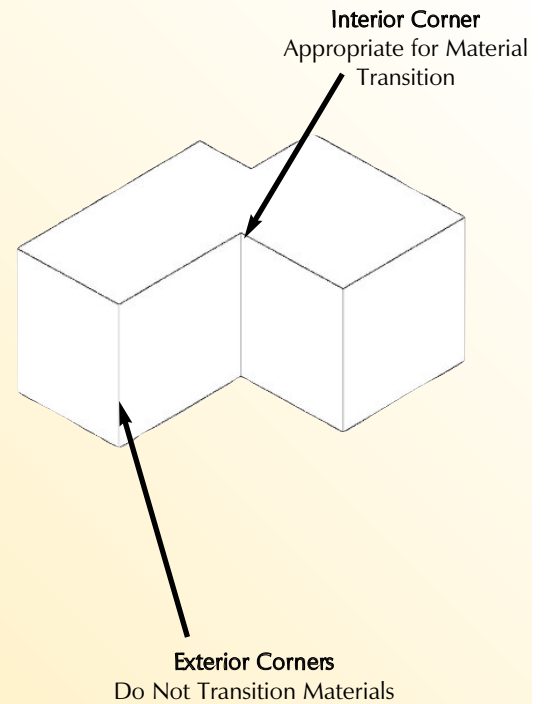


- **Ornamental Metals** are acceptable as accent materials such as grills, railing, panels, gutters, etc.
- **Concrete Masonry Unit / Decorative Concrete Block** is acceptable only when used in non-visible areas or as a secondary building material or accent (i.e. banding). It is considered most appropriate as material found in the “base” portion of building. The use of standard, plain gray block or glazed block is not acceptable. All other uses will be reviewed on a case by case basis.

Material Changes

Material changes should always occur at inside corners to give the materials a sense of permanence and thickness. A change at an exterior corner does not provide this sense and additionally, may provide an unprotected seam vulnerable to damage and peeling. In most cases, it is recommended that primary facade materials should “wrap around” exterior corners.

Material changes may also occur with horizontally. In this case, it is generally recommended that the heavier material appears below the lighter. Generally, changes of material should include a minimum 1/2” variation in wall plane.



2.4 Facade Composition

As discussed in the general building guidelines (Section 2.1) buildings should be comprised of a distinct base, a middle, and a top. All visible sides of a building shall include materials and facade treatments that complement the front facade.

Front Elevations - Non-Residential

1. Commercial uses shall have at least 60% of their ground floor front elevation with transparent windows. Window coverage should not exceed 90% of the ground floor elevation.
2. In instances in which the main entrance or front facade does not face the street, the street facing elevation shall be designed as a front elevation.
3. Upper stories shall not exceed 60% window coverage.

Side Elevations: Non- Residential

4. Side elevations that face a primary street should be designed as front elevations. Other building sides that are visible from walkways, or similar public spaces shall utilize primary and secondary materials that are required for front facades.
5. Visible side elevations shall incorporate the use of other scale providing features including horizontal banding, columns, sills, lintels and other features to emphasize window openings, changes in color, material, or texture.

Residential Composition

Facades of condominium and other residential construction should also follow the following:

6. Materials should follow Section 2.3.
7. Window coverage should range between 15% and 50% of the facade.
8. To enforce privacy, residential entrances should be elevated from the public sidewalk.



Sides and Rears of Buildings. The back and sides of this building front a busy street. The entrance and parking is located in the rear. To address the street, this building has a masonry facade, banding, and includes false windows.



Rear Elevations

It is important that all sides of the building visible from the public streets (or other public spaces) are designed to complement the front facade. Redevelopment projects and the location of the Inner Urban Trail increase the visibility of these areas. Thus, the appearance of rear elevations takes on great significance throughout the Town Center.

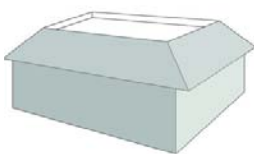
9. On buildings where the rear elevation faces the street, the rear facade should be treated as a front facade.
10. On other buildings, when the rear elevation is highly visible to the public, such as parking areas, bike trails, walkways or other public or quasi public space, the rear elevation should be given design treatment as if it were a visible side facade.
11. Rears of buildings may include porches, decks, signage or patios, or special design elements.

2.5 Building Height

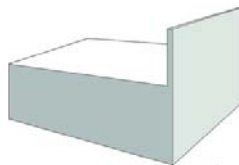
As stated in the general guidelines (Section 2.1), building height adds character to the street.

1. The building height limit is three and 1/2 stories. A half (1/2) story indicates a floor level above the eave line, or a floor level less than 50% of the area of the floor below.
2. The minimum building height is 1 and 1/2 stories. New one story buildings should contain extended parapet walls, gabled roof forms or similar treatments to give the appearance of a taller building. Parapet walls should be designed in a manner to appear as a solid, three-dimensional form, rather than a veneer. This includes wrapping the parapet around the front corners of the building.

Encouraged Parapet Treatment



Discouraged Parapet Treatment

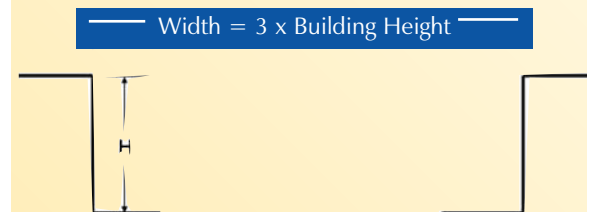


Rear of Buildings are Important Design Considerations.

As opportunities for shared rear parking and lot access increase, careful design consideration should be given to the rears of buildings. The above sketch shows awnings, plantings, and site lighting that improves the appearance of the backsides of buildings and properties

How Tall should Buildings be?

Generally, streets should be 2-3 times wider than buildings are tall. This provides a pleasing scale to the street. With a 66 foot right-of-way, such as the one on Main Street, buildings should be between 2 and 3 stories to meet this guideline.



Buildings with limited height or large setbacks from the street add little to the character and definition of the street edge. Conversely, buildings that are too tall will appear out of place with existing structures as well as having other potential detriments including limiting sunlight.



2.6 Roof Forms and Materials

The following roof forms are allowed in the Town Center District.

Preferred Roof Forms



Gable



Hip



Combined Flat with Hip or Gabled

Discouraged Roof Forms

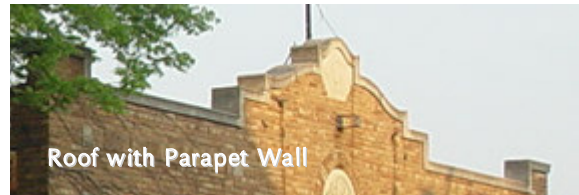


Gambrel



Mansard

1. **Gabled Roofs.** These roofs should have a minimum 8 inches rise for 12 inches of run. The gable end should face the primary street.
2. **Hip Roofs.** When done in conjunction with dormers that face the primary street.
3. **Flat Roofs with extended and shaped parapets.** Parapets are extended walls that give height and shape to the roofline. These are an acceptable roof expression. Any parapet wall should be designed in a manner to appear as a solid, three-dimensional form, rather than a veneer. In addition to enhancing the architectural character of the roof, parapet walls should screen all rooftop mechanical equipment.



4. **Combination Flat and Gabled Roofs or Hip Roofs.** Flat roofs are acceptable on commercial and larger buildings when done in conjunction with gabled roofs, hip roofs dormers, or articulated parapet walls. If a long roof paralleling the street is unavoidable, large gables facing the street should be used to help maintain the rhythm of gables facing the street.
5. **Discouraged Roof Form.** Gambrel, Flat and Mansard roof forms are discouraged.

6. **Acceptable Roofing Materials.**

Roofing material choices would likely differ between the design districts. The historic district would accommodate more traditional materials listed below. The materials should be similar in proportion and size.

- Clay Tiles
- Wood Shingles
- Slate
- Asphalt Shingles
- Metal Tiles

The roofing materials in other areas can include, in addition to the above:

- Metal - Copper Coated, terne metal, or painted to resemble weathered copper
- Roofing Tile - Cement roof tiles in natural colors
- Aluminum

The use of “green” roofs should be highly encouraged in newer buildings in which flat roofs are used. They are environmentally friendly, and help reduce many adverse effects such as heat islands etc.

7. **Discouraged Roof Materials.** Materials that were not historically present and do not relate to the remaining historic buildings should be avoided. (i.e. Clay tiles should not replace wood shingles.) Materials such as PVC and Rubber should be discouraged due their toxic nature and general aesthetics. Tar and bitumen roofing should also be avoided as they have a short-life, are messy to install and have limited visual appeal.

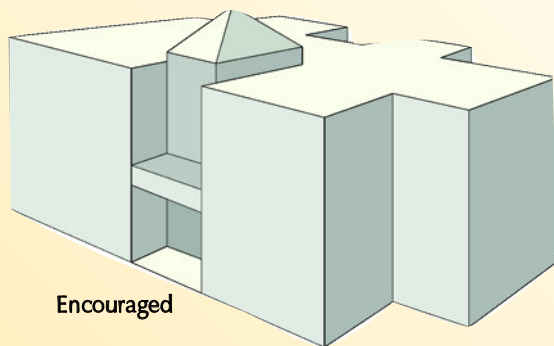
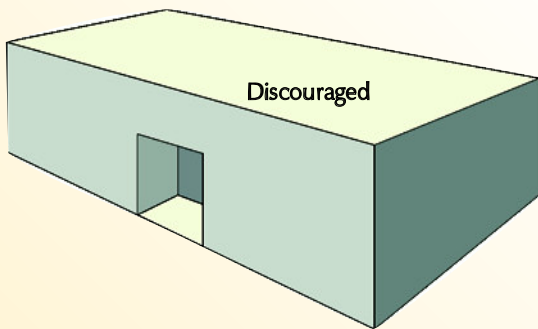
2.7 **Corner Buildings**

Corner buildings should define intersections with distinctive architectural character, particularly at major crossroads. To provide for this character, designers should include features such as towers, rounded walls, recessed entries or other unique features. Additionally, corner buildings should have minimized setbacks. Small setbacks, normally less than 10 feet, would be allowed to provide plaza, landscape, outdoor seating space, or similar feature to enliven the streetscape.



2.8 Larger Development and Redevelopment Projects

1. New buildings occupying in excess of 25,000 sf and/or 60 feet or more of street frontage should be designed with recesses and projections, material changes, or other articulation every 30 to 60 feet, to break up large masses and create the appearance of smaller buildings and individual storefronts.
2. Incorporate the use of other scaling features including horizontal banding, columns, sills, lintels and other features to emphasize window openings, changes in color, material, or texture.
3. Smaller retail stores that are part of a larger principal building should have display windows and separate outside entrances.
4. No more than 15% of the display window can be obstructed by signages and/or other facade elements. The remaining area should be clear and unobstructed.
5. Flat roofs are acceptable on commercial and larger buildings when done in conjunction with gabled roofs, dormer or articulated parapet walls. If a long roof paralleling the street is unavoidable, large gables or similar treatments facing the street will be required.
6. Include sidewalks along the full length of any facade featuring a customer entrance.
7. Include internal pedestrian walkways that are marked by changes in grade, material, or color.



Variations in roof and facade lines improve the form and scaling larger-format architecture

2.9 Gas Stations & Small Franchise Architecture

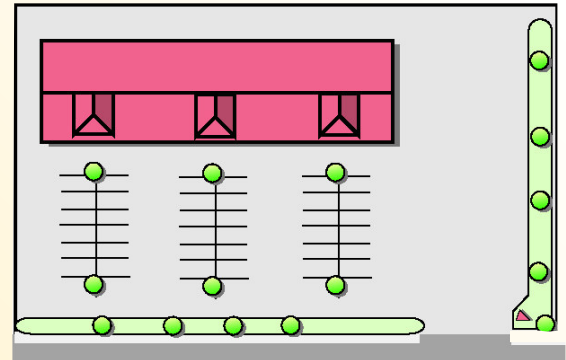
1. Incorporate masonry or compatible building materials into the pump stations.
2. Place stores close to the street edge and pump stations to the side or rear.
3. Encourage franchises to modify their typical designs to blend with the overall Town Center character.
4. Incorporate the use of awnings, varied roof forms, roof lines, and other elements that provide scale.
5. Follow Screening Guidelines in Section 2.11



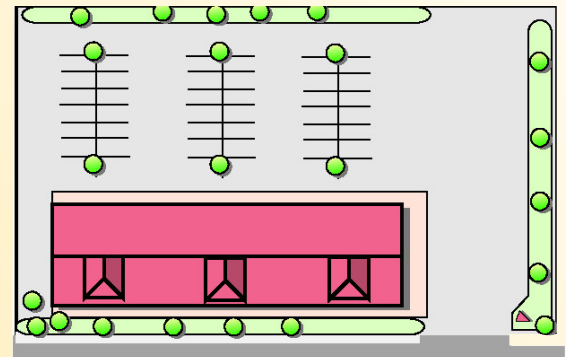
2.10 Multi-Tenant Commercial Buildings

Main Street, Cedarburg Road, and Mequon Road currently are home to several multi-tenant commercial buildings. These developments often take the form of the conventional “strip-center”. Such developments typically have little building frontage on the street edge, and rather large parking areas found between the building and the street. It is recognized that the multi-tenant building is an important part of the overall use mix. Considering this, this section offers the following recommendations.

1. The design of new centers should follow the building and site guidelines found elsewhere in this report.
2. Site designs should bring the building at or near the street edge and parking to the rear. Site designs in which the side of buildings front the street are a recommended alternative, provided the street facing portion of the facade has front-facing details as discussed in the materials and composition section.
3. New centers should incorporate a vertical mix of uses that include lower floor retail and service along with upper-story residential or office uses.



Conventional Multi Tenant Building Design



Preferred Design Concept



2.11 General Site Design Guidelines

Site design refers to the physical arrangement of buildings, walkways, parking lots, lighting, landscaping and other elements on a property. The arrangement of these elements contributes to the functional and aesthetic character of the site and ultimately, the entire district. Site design that places attractive buildings closer to the street strengthens the character of the street. In contrast, sites with large unscreened front parking areas or large setbacks generally contribute little to the aesthetic quality or the pedestrian environment of the district.

1. Bring new construction or additions close to the sidewalk.
2. Buildings should be situated parallel to the street.
3. In cases where buildings are set back, add dense landscaping or decorative fencing to help define the street edge.
4. Move parking to the sides and rears of buildings. Corner lots should screen parking through building placement and adequate landscape treatments.
5. All parking lots should be screened with ornamental fencing, dense hedges and other plantings to soften the visual impact.
6. Shared parking areas between adjoining properties are encouraged to limit curb cuts.
7. Connect entrances to the public sidewalk.
8. Promote downward aimed site lighting that complements the public street lighting
9. Screen trash and utility areas through landscape design or decorative fencing. Separate these uses away from public entrances or public walkways



Screen Parking Areas If it is not possible to bring buildings up to the street edge, landscaping or decorative fencing should be used to reduce the visual impact of surface parking and enliven the streetscape.



Illustrations of General Site Design Guidelines

Plan illustrating methods to conceal parking at street intersections:

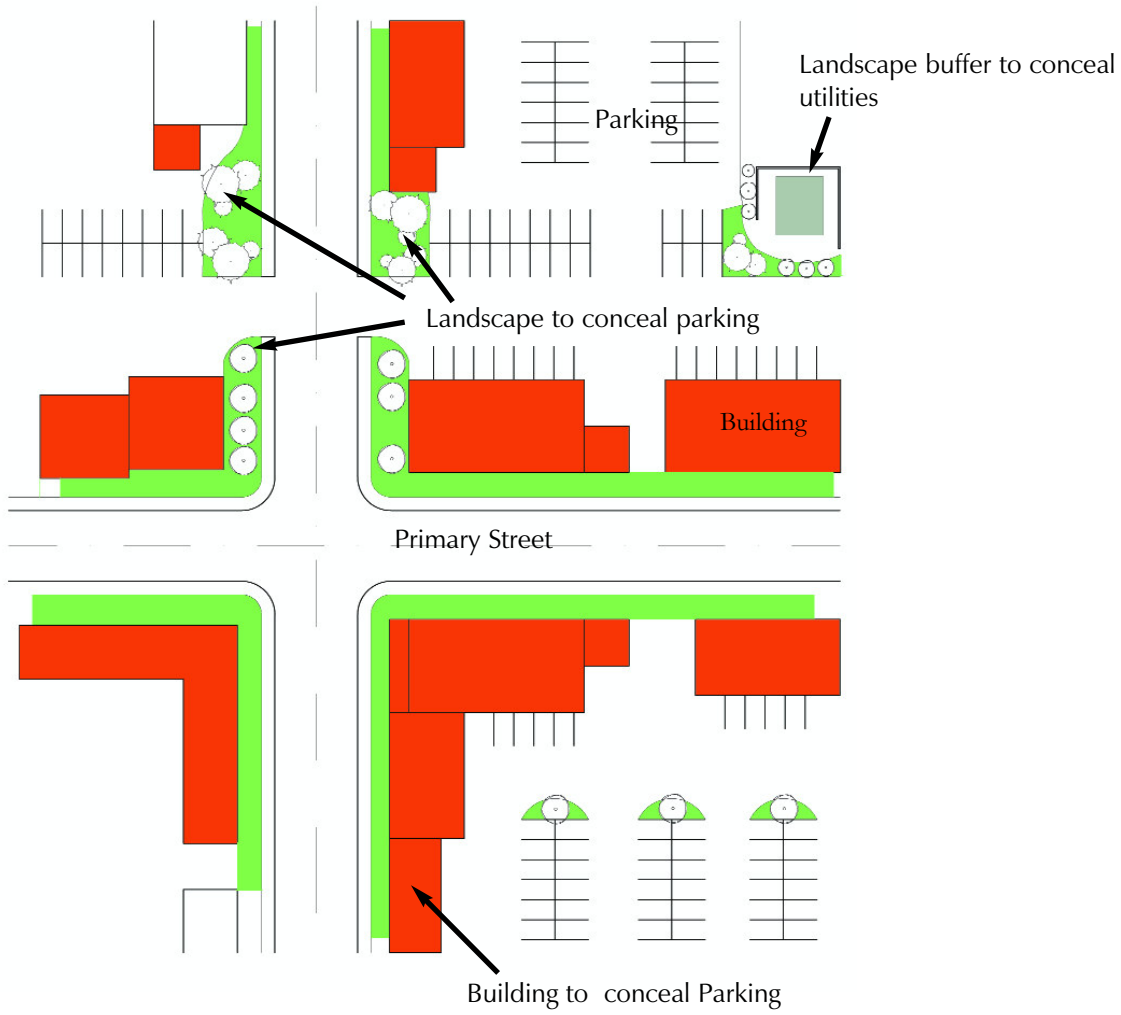
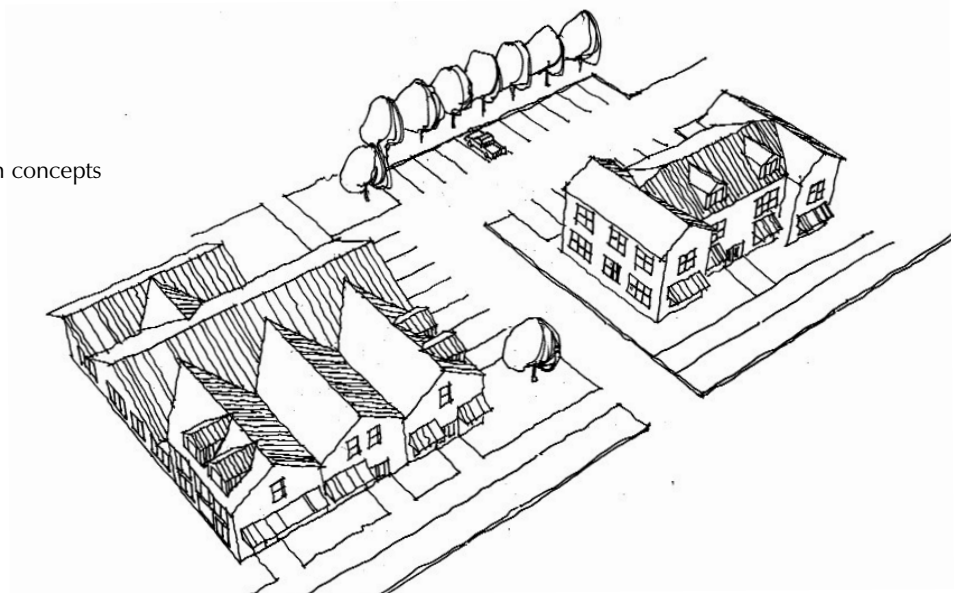


Illustration of Preferred site design concepts



Signage Considerations

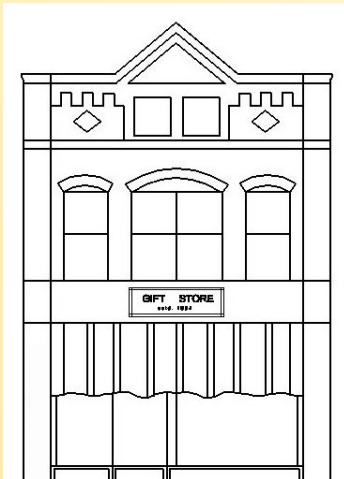
Where a sign is mounted is as important as the style of the sign itself. Shop front signage must be located within specific zones on the front of the building in order to capture the attention of pedestrians and motorists and to complement or, at least not detract from, the architecture of the building.

One of the most important things to keep in mind about signage is that one well designed, appropriately sized and located sign is better than five poorly designed signs. It is a great mistake to clutter the front of a building with multiple signs, as is often done on the mistaken premise that the more signs a business has the more the public will be attracted to it.

In fact, just the opposite often occurs, since people are confused and repelled by signage that tries to convey too much information or creates cluttered, disorganized image of a business. When signage becomes too loud or abundant, it no longer works to attract patrons and tends to provide mere visual pollution. When businessmen begin to compete with each other by trying to outdo their neighbors' signage with more and bigger signs, the entire visual character of a commercial area can be changed to a tawdry cheap image that may actually hurt business.

The modern, internally-lit, plastic box sign that is widely used today is strongly discouraged. This box sign is composed of a translucent plastic facing attached to a metal box frame that contains lighting equipment. Box signs have a bulky, modern, machine like appearance that looks out of place in much of the study area.

*-- From the Thiensville Architectural Guidelines,
Miller Architectural Group*



Signages placed at the center of front facades lend a better balance to the building, primarily because the architecture of most buildings is based on symmetry.

2.12 Signage

Building signage should fit with and enhance the character of a building and the overall Town Center area. As buildings in the corridor exhibit a broad period of architectural history, signage should be reviewed based on three major design components:

Style

The style is comprised of the type color, material, font and background of the sign itself. Appropriate signage types include:

- Awning Sign
- Decal or Vinyl on glass
- Hanging Sign - Mounted on Brackets
- Leaded or art glass transom lettering
- Monument, architecturally integrated signs (Contain materials and design elements found in serving building.)
- Raised or painted letters
- Sandblasted or etched structural glass
- Wall mounted cast bronze plaques or tablets

As discussed in existing sign ordinances, no flashing, scrolling, moving signs shall be permitted. Backlit plastic signs are highly discouraged, especially in the historic district. External illumination of signs from projecting light sources is encouraged. Individual back-lit letters may be appropriate outside the historic district.

Content

Business signs shall only include the name of the business, nature of business and the street address.

Location

- Signs should generally be centered within the prescribed signage area of the building. Signs should not mask upper floors or architectural details.
- Window signs should be allowed in storefronts. The size of the sign should not account for more than 20% of the window size. Signage hung in window openings shall not obscure more than 15% of the display space.
- Projecting signs shall not be located above the base level of the building or above the second floor line. Hanging signs shall not project more than 40" total from the face of the building.



3. Design Districts

Design Districts

The specific design guidelines are organized around three distinct design districts. Each district has its own character and plays a unique role in the larger Town Center.

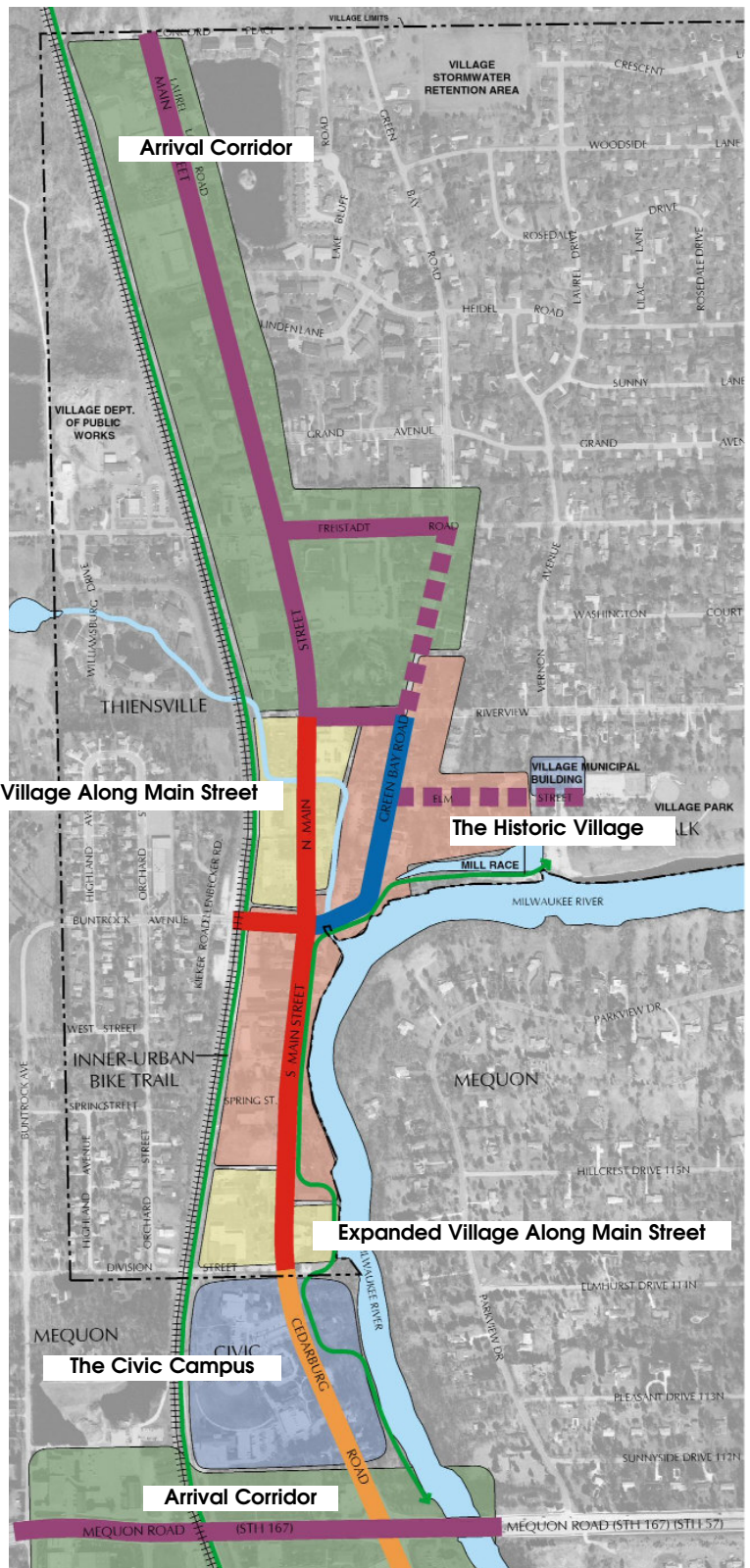
Design guidelines for each district should be considered in combination with the general guidelines discussed in part two and other adopted ordinances.

The following design districts have been established:

- A. The Historic Village
- B. Expanded Village Along Main Street
- C. Arrival Corridors

Comments on the Civic Campus

Design considerations for the civic campus focus on the public realm and public streetscape. No private development guidelines are included in this section as no private redevelopment is proposed.



4. Specific Design Guidelines for the Historic Village

The Historic Village marks the center of the study area. The district's historic character is among the most recognizable assets and community strengths. Preserving this character and promoting compatible infill development should remain a top priority. Design in the Village should provide clues to future improvement and redevelopment projects throughout the Town Center.

Design Opportunities For The Historic Village

- **Protect the existing character and support compatible infill development.**
- **Continue to enforce the existing Architectural Guidelines in this District**
- **Promote physical and visual access to the Milwaukee River**
- **Promote pedestrian connections through parking areas.**

Historic Village Desired Character Images

- **1 1/2 - 3 1/2 Story Buildings**
- **Bring Buildings Up to Sidewalks.**
- **Buildings should frame streets.**
- **Promote shared parking behind buildings.**
- **Divide larger buildings into smaller bays to resemble smaller buildings.**
- **Promote variations in roof lines**
- **Utilize landscaping and parked cars to buffer people from traffic**
- **Promote street level activity**
- **Create transparent storefront windows**



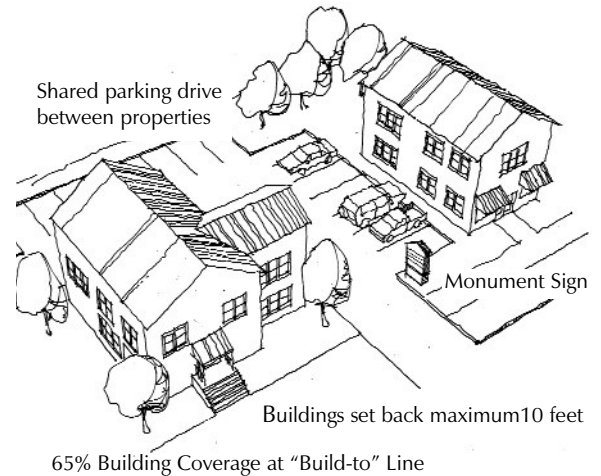
4.1 Existing Guidelines

1. Bring buildings close to the right-of-way (10 Foot Maximum Setback).
2. Encroachment zone (0-8 feet) used for bays, porches, balconies, stairs, etc.
3. The front of the building should occupy 65% of lot frontage, at the setback / build-to line.
4. Parking is not allowed in front of buildings. Parking should be located on interior sides and rears of buildings. Parking on corners should be screened through buildings and landscaping.
5. Gabled roofs, hip roofs are preferred roof forms (These may be done in conjunction with flat roofs).
6. Articulate entryway (Utilize Awnings, porches, recesses)
7. Utilize vertically oriented windows (Taller than wide)
8. Color is a subjective choice, but should be selected from an appropriate "Historic" color palette.
9. Promote a rectangular building shape with short end facing the street.

4.2 Promoting Shared Parking and Access

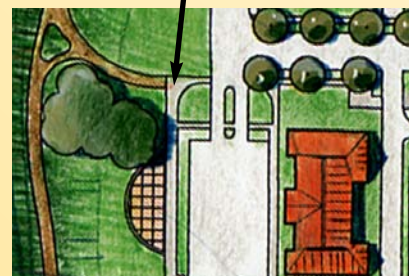
One of the opportunities associated with the Historic Village and the Expanded Village at Main Street District is to promote shared parking. As property owners are willing, or as larger redevelopment projects are proposed, the following should be considered:

1. Provide access between public or shared private areas to promote better vehicular and pedestrian circulation.
2. Promote access to the inner urban bicycle trail.



The sketch above illustrates many of the guidelines in place for the Historic Village District. This includes minimized setbacks, varied roof forms, shared parking, monument business signage, and landscaping.

Rear parking linked to public bike trail



4.3 Additional Material Considerations

The following guidelines apply to the restoration and rehabilitation of existing structures in the existing Historic District.

1. Acceptable primary materials include all materials described in section 2.3 and wood siding in select cases. Wood siding is appropriate as part of a historic rehabilitation project, in which wood siding was the original material. Wood siding may also be appropriate for infill projects in the historic district in which the adjacent structures have wood facades and are contributing structures to the historic district. Thus, wood siding is allowed as a primary material when it fits within the context of the site and block.
2. Materials that were not historically present and do not relate to the remaining historic buildings should be avoided. (i.e. Vinyl siding should not replace wood siding).
3. Brick, stone, and wood used in new buildings or rehabilitation projects should match such materials used elsewhere on the building in texture, color, and coursing.
4. Modern materials that are included in the allowable materials list may be introduced, however the sizing, texture, colors and transitions shall be implemented in a manner that complements adjacent historic properties.
5. Original sills, lintels, frames, sash and mullions should be preserved. Necessary replacements should duplicate the original design and materials.
6. The application on non-authentic details should be avoided. (This includes false shutters, balconies, etc)

4.4 Additional Guidelines for Infill Construction

New construction shall be designed to respect the context of the block and the remaining historic construction. Construction shall result in a building scale that “fits in” with the surrounding structures. Setbacks and building heights should remain consistent with surrounding buildings. Similarity, architectural massing and rooflines should follow clues from existing buildings.



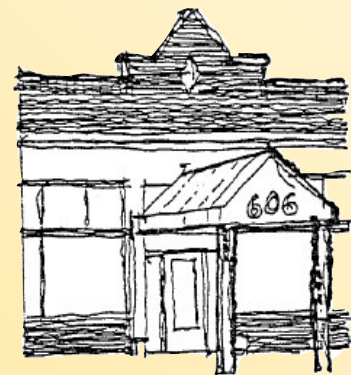
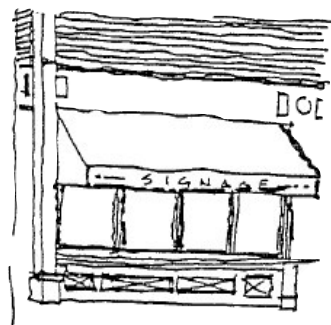
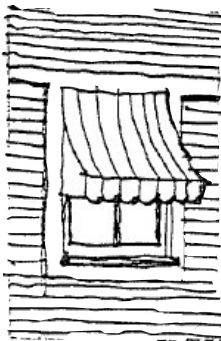
4.5 Canopies and Awnings

Canopies

1. Fabric Canopies are acceptable. Building supported canopies are allowed to project up to 4 feet beyond the street edge property line. Canopies supported by poles are allowed within encroachment zones.
2. Rigid Canopies are allowed, provided the wall to which the canopy is attached extends a minimum of 4 feet above the top edge of construction and materials to match and complement the building to which they are attached.

Awnings

1. Awnings over doors and windows are encouraged, provided they are an integral part of the architecture of the building and reflect the design and character of the structure.
2. Plastic or plastic laminated awnings are discouraged in the Historic Village district.
3. Waterfall or bubble type awnings are discouraged. Awnings should be a straightforward shape of fabric. Signage along the front edge is encouraged. Retractable awnings should be considered.



The sketch above illustrates the acceptable types of awnings: The entry Awning, canvas store front awning and the canvas window awning respectively.



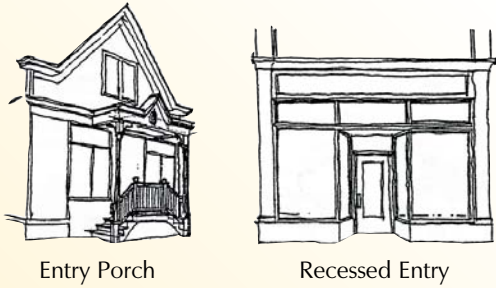
Undesirable Waterfall, or Bubble Type Awning



4.6 Other Building Considerations

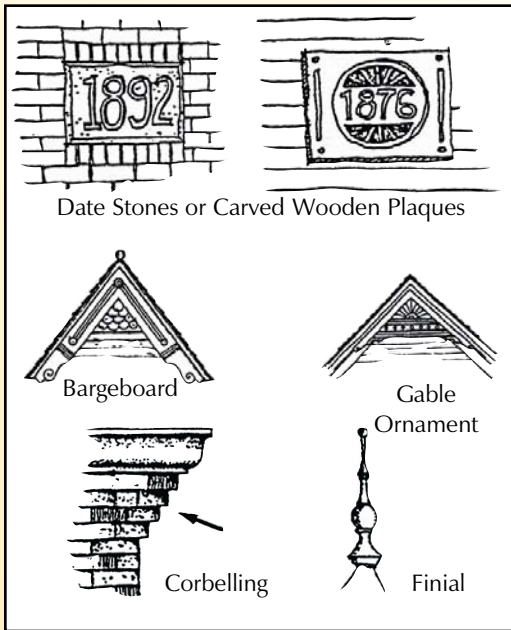
Decorations

Decorative elements have commonly been incorporated into many buildings and are encouraged to be included in any projects. The following examples illustrate some of the many ways in which decorative architectural elements, in various materials, can be used.



Entry Porch

Recessed Entry



Date Stones or Carved Wooden Plaques

Bargeboard

Gable Ornament

Corbelling

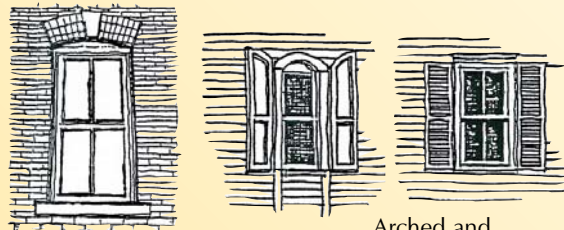
Finial

Decorative Architectural Elements

Windows

All primary windows are to be vertically oriented with a minimum of 2 units height to each unit horizontal. The use of double hung or casement windows is strongly encouraged. All windows at the second story or above are to be operable. It is also strongly encouraged that any windows at ground floor (as opposed to storefront) should be operable. Natural finish (mill finish) aluminum storm/screen combination units are not acceptable. Please note that these requirements do not include retail storefronts.

Sliding windows are strongly discouraged. Windows may be wood, vinyl clad wood, aluminum clad wood, or aluminum. Aluminum windows are not acceptable. Colors are to coordinate with and complement the historic color palette. Window openings are to be articulated; for instance, with sills and lintels in masonry walls, or with wood casings and ornamental lintels in wood framed or sided walls. Window openings are preferred to be rectangular, but segmental arch and round head windows are acceptable.



Articulated Lintel and Sill

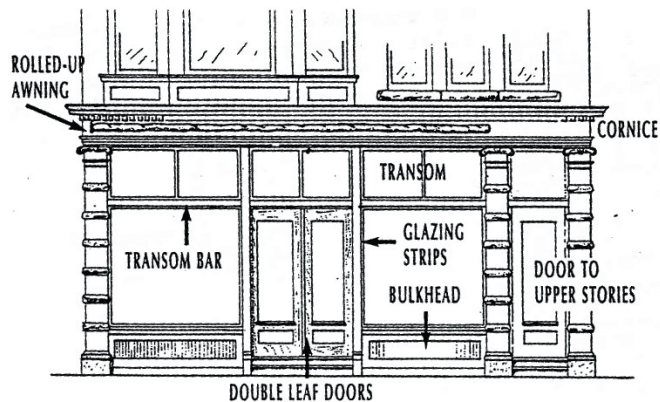
Arched and Rectangular Top



Keystone Arched



Varied Window Groupings



Building Cleaning

Under no circumstances is it allowable to clean masonry by sandblasting. Chemical or mechanical cleaning should be used.

Painting

Painting gives the façade a well maintained appearance and is essential to ensuring a long life for many traditional materials. The steps below should be followed to ensure a quality job.

1. Catalog all the façade materials painted. Brick, wood and metal have different paints and procedures. Consult a local expert for advice. Use only quality paint products.
2. Make any necessary repairs to surfaces before starting; replace rotten wood, repaint masonry mortar joints, remove rust from metal etc.
3. Carefully prepare each surface in accordance with the manufacturer's instructions for the paint being used. This will include scraping, sanding, and a thorough cleaning. Good surface preparation is an essential step to ensure a good looking, long lasting finish.
4. Apply the paint in accordance with the instructions. Paint only in satisfactory weather, and plan to use a primer as a first coat for better surface adhesion. Follow with two coats of the final color.

Color

The amount of sun can change the appearance of a paint color, paint chips should be checked on both sunny and cloudy days. Painting a small section of the building in the chosen colors is the best way to check the effect of the colors on the building.

If some basic color and paint guidelines are kept in mind, color can add the visual richness and appeal of Thiensville's Village Center. Paint color choices should be limited to the paint color included in Pittsburgh Paints or Benjamin Moore Paints Historic Paint Collections.

The color of the upper wall surface and the store-front piers is the base color. In most cases, if these elements are masonry and are not currently painted, they should not be painted.

Paint should never be removed from old masonry by sandblasting or abrasive cleaning. If the masonry or wood wall is to be painted, the choice of base color is of primary importance. Generally, light base colors will visually recede and emphasize the trim.

A typical 3-color design includes:

1. A base color for the siding (for unpainted masonry walls, the natural brick or stone is the base color).
2. A major trim color for prominent details such as cornices, window enframements, and corner boards.
3. A minor trim color used sparingly for window and other small decorative

Keep these points in mind:

- If the building was built before 1900, the window sash, the movable parts of the window unit, were usually painted the darkest color used on the building.
- Too many colors can create a confused, clumsy appearance. Apply paint to a "test area" and evaluate the effect before making any final decisions.
- Use the accent or minor trim color sparingly.



5. Specific Guidelines for the Expanded Historic Village at Main Street

The following design guidelines provide a framework for infill and redevelopment projects along Main Street, adjacent to the historic district. The intent is to complement the historic character, but not recreate it. Features such as varied rooflines and a strong street presence should be encouraged. Additionally, these guidelines provide guidance on how new and existing buildings can work together to create coordinated blocks that enliven the street.

Design Opportunities For The Expanded Historic Village along Main Street

- Improve the pedestrian character of Main Street.
- Create a framework for larger/anchor redevelopment that complements, but does not replicate, the Historic Village character.
- Create coordinated blocks.
- Provide for convenient parking but reduce its visual impact on the streetscape.

Expanded Village Desired Character Images

- 2 - 3 1/2 Story Buildings
- Bring Buildings Up to Sidewalks.
- Buildings should frame streets.
- Move parking to shared parking behind buildings.
- Individual buildings create attractive, coordinated blocks.
- Divide larger buildings into smaller bays to resemble smaller buildings.
- Promote variations in roof lines
- Create a comfortable pedestrian experience by buffering sidewalks with landscaping and on-street parking.
- Promote street level activity
- Create transparent win-



5.1 Additional Architectural and Building Guidelines

Architectural standards for this design district are found in Chapter 2, sections 2.1 -2.9. No additional standards have been created for this district.

5.2 Additional Site Design Guidelines

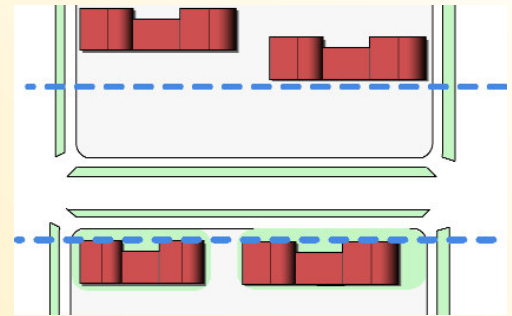
The general site design guidelines, found in section 2.10 apply to this district. In addition, the following standards apply:

1. Redevelopment Projects along Main Street and Riverview Street should utilize the concept of "build-to" lines versus setback lines. A build-to line establishes the point to which buildings should be placed. This is in contrast to the more conventional zoning concept of "setback" lines that dictate how far a building must be placed off the front property line.

As redevelopment projects are considered, a build-to line should be established in the block. Depending on the specific proposal, this build-to line should be set between 0' and 10' from the public sidewalk. Infill projects should place the build-to line equal to adjacent buildings, when part of an already established block face.

5.3 Creating and Enhancing the Blockface

1. Where new buildings are designed for existing block-faces, the building setback should be consistent with adjoining buildings.
2. Individual buildings should create coordinated block-faces.
3. Buildings should be built up to the build-to line. This standard would not preclude minor variations to break up the front plane of a facade into smaller bays.
4. For blocks, a minimum of 65% of a block should have building frontage at the "build-to" line.
5. Parking should be located behind buildings or along their interior side.



Conventional Setback Line (above), requires buildings to be setback a predetermined distance from the street.

Build-To Line (below), requires buildings to be built up to a certain point on the street edge, typically to place the building closer to the street edge. This helps to frame and enliven the street.



The Blockface

Individual buildings form blockfaces. The blockface can be considered a "wall" to an outdoor room. Blocks and block faces can take on a variety of forms.



6. Specific Guidelines for the Arrival Corridors

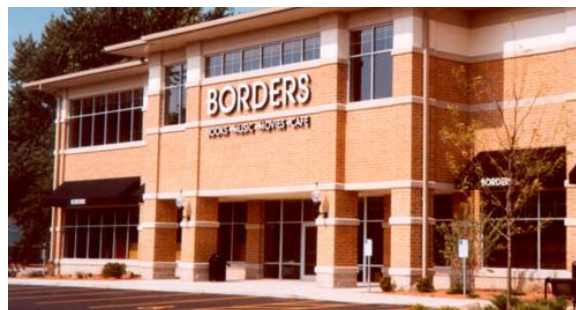
The Arrival Corridors mark the entrances and the first impressions into the Mequon / Thiensville Town Center. The intent of this district is to promote design that exceeds that of the typical commercial strip development. These guidelines provide a framework to create signature entranceways as new construction, additions, and redevelopment projects are brought forward.

Design Opportunities For The Arrival Corridors

- Create distinct and attractive corridors that announce arrival into the Town Center.
- Minimize the visual impact of parking.
- Complement the area's historic character, but do not recreate it.

Arrival Corridors Desired Character Images

- 1 1/2 - 3 1/2 story buildings
- Bring buildings closer to the sidewalks.
- Move parking to interior sides and potentially rears of buildings.
- Create and maintain a unique entrance
- Articulate larger buildings.
- Promote variations in roof lines
- Improved standard franchise design



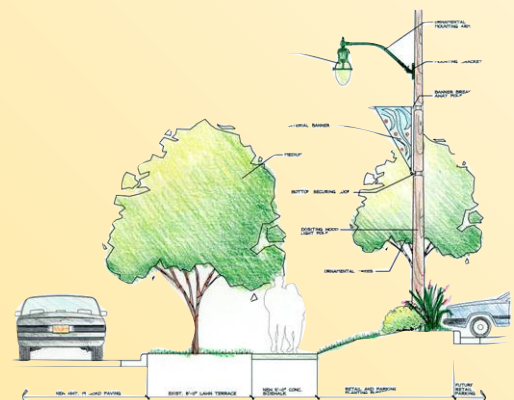
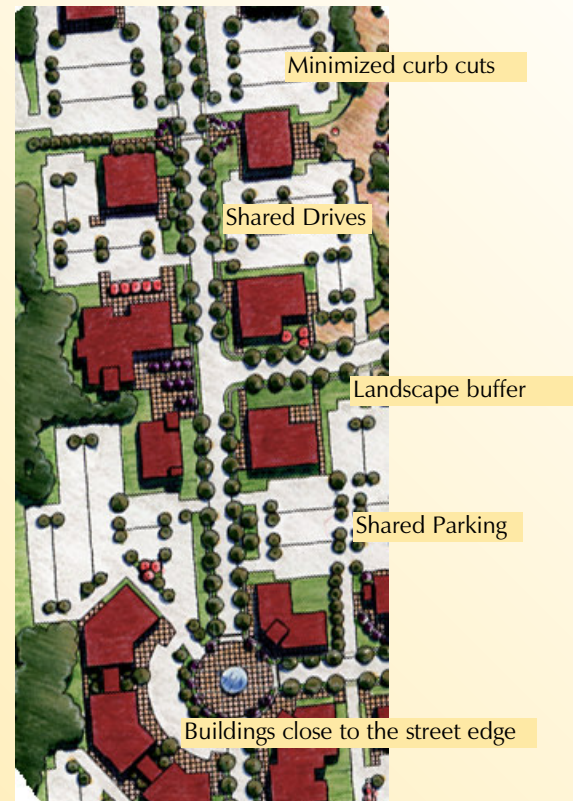
6.1 Additional Architectural and Building Character

Architectural standards for this design district are found in Chapter 2, sections 2.1 -2.9. No additional standards have been created for this district.

6.2 Additional Site Design Guidelines

The general site design guidelines, found in section 2.10 apply to this district. In addition, the following standards apply:

1. A minimum landscaping area of five feet should be provided between any surface parking and the right-of-way line.
2. Follow applicable zoning regulations regarding interior side and rear setbacks.
3. Buildings are strongly encouraged to be moved towards the street edge. Setbacks should generally not exceed 10 feet, however, larger setbacks may be appropriate on Mequon Road due to its wide right of way width.
4. Internal walkways should be provided to interconnect parking lots with building entrances.
5. Walkways should be provided to connect the main building entrance to the public sidewalk.
6. Locate large parking areas to the side or rear of buildings, rather than in front yards of buildings, whenever possible.
7. Buildings should have a prominent location at key intersections. Parking should be moved to other sides of the building so that the building and landscaping define the corner, not parking lots.
8. Parking areas should be well landscaped and lit with downward aimed lighting that complements the public streetscape.
9. Whenever possible, adjoining parking lots should be linked to provide internal traffic circulation.



In situations in which parking in front of the building cannot be avoided, a landscaped buffer should be provided between the parking area and the public sidewalks

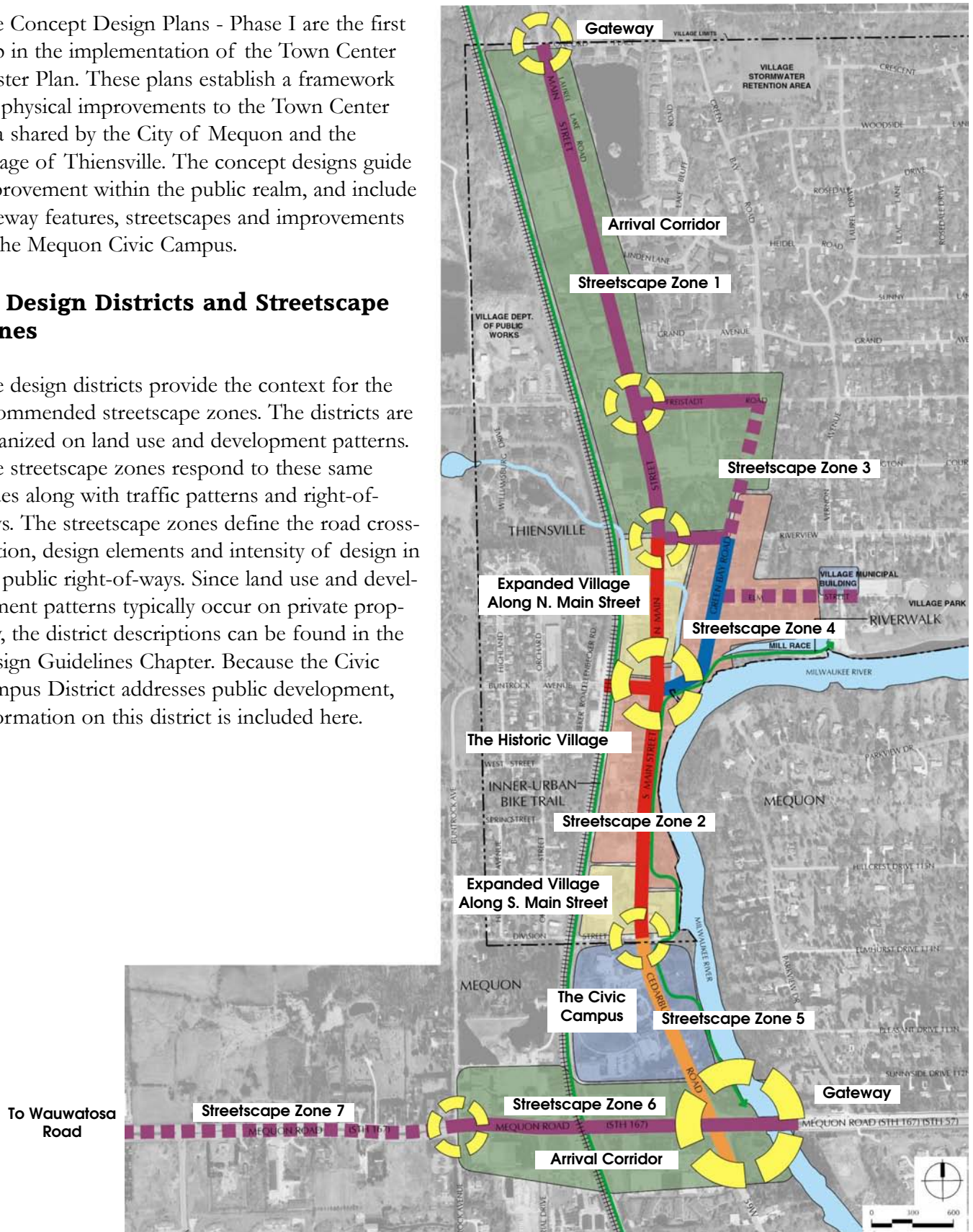


Chapter 2: Concept Design Plans - Phase I

The Concept Design Plans - Phase I are the first step in the implementation of the Town Center Master Plan. These plans establish a framework for physical improvements to the Town Center area shared by the City of Mequon and the Village of Thiensville. The concept designs guide improvement within the public realm, and include gateway features, streetscapes and improvements to the Mequon Civic Campus.

1. Design Districts and Streetscape Zones

The design districts provide the context for the recommended streetscape zones. The districts are organized on land use and development patterns. The streetscape zones respond to these same issues along with traffic patterns and right-of-ways. The streetscape zones define the road cross-section, design elements and intensity of design in the public right-of-ways. Since land use and development patterns typically occur on private property, the district descriptions can be found in the Design Guidelines Chapter. Because the Civic Campus District addresses public development, information on this district is included here.



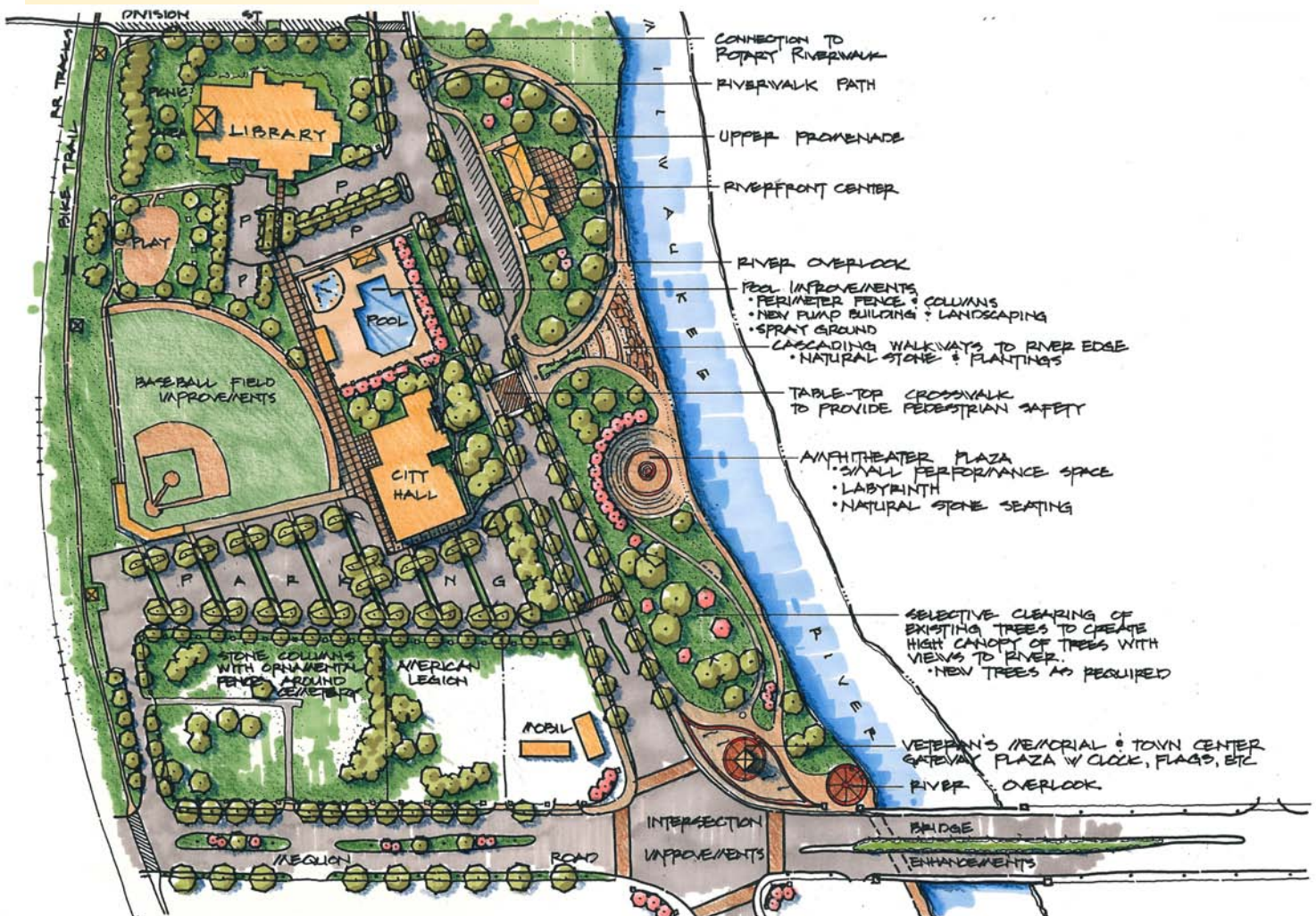
2. Design District D - The Civic Campus



Mequon City Hall

The Civic Campus design concept plan provides a framework for improvements adjacent to Cedarburg Road from Mequon Road to Division Street. The Civic Campus area also includes the current Mequon Civic Campus area that encompasses the Mequon City Hall, Public Library, Public Swimming Pool, Logemann Community Center, Rennie Field, the G. Opitz Cemetery, and a section of the Inner Urban Bike Trail. The proposed Civic Campus District expands the current campus to include a riverfront park on the east side of Cedarburg Road that will include a major Town Center entrance feature at the northeast corner of the Mequon Road and Cedarburg Roads intersection. A veteran's memorial will be part of this gateway feature. The list below describes other Civic Campus improvement opportunities to be phased in over time as funds become available.

Civic Campus Design Opportunities



- Enhance the existing pool area.
 - Provide new restroom facilities and pump building adjacent to the pool.
 - Provide new spray ground adjacent to the pool.
 - Replace existing perimeter fencing with ornamental wrought iron fence with brick/stone columns and landscaping.
- Improve Civic Campus with additional landscaping of existing parking lots and buildings.
- Develop new Civic Campus identity signs.
- Construct a major entrance feature at the intersection of Mequon and Cedarburg Roads. The feature should include a veteran's memorial, clock tower, special paving, flagpoles, overlooks with decorative railing, interpretive signs, and other design elements.
- Re-orient and reconstruct Rennie baseball field with home plate located at the southwest corner of the site. Include improved perimeter fencing around the ball field at time of reconstruction. The relocation requires the demolition of the Logemann Community Center.
- Expand parking on the south side of City Hall and the ball field to accommodate 250+ cars. Parking lot will have islands, decorative lighting and landscaping.
- Improve the perimeter of the cemetery with brick/stone columns, ornamental wrought iron fencing and gateway entrances.



Frank L. Weyenberg Library



Rennie Field



Playground in Civic Campus



Spray Park Example



Mequon Pool



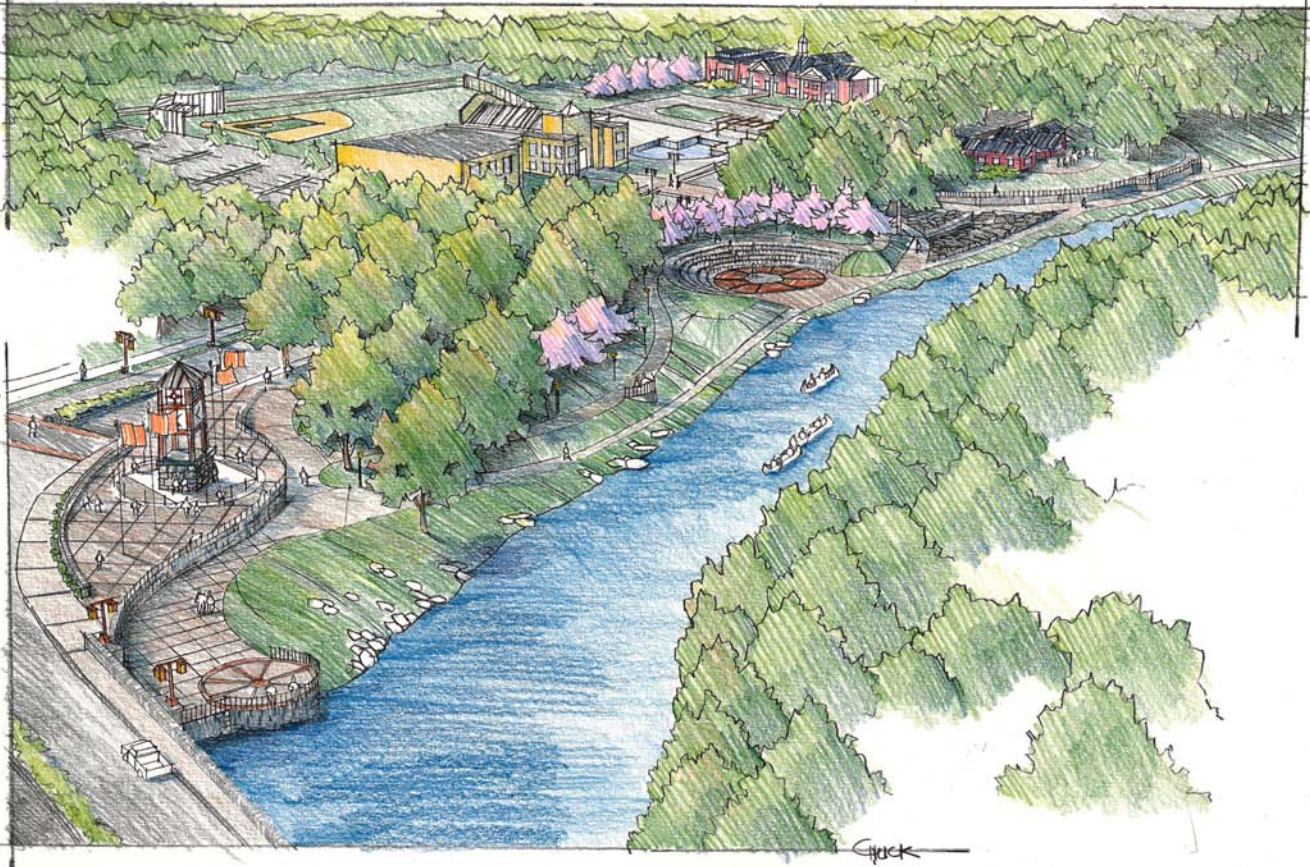


Labyrinth Example



Amphitheater Example

- Expand Civic Campus to include a riverside park between Cedarburg Road and the Milwaukee River. The park will improve visual and physical access to the Milwaukee River. Proposed park improvements include upper and lower pedestrian walkways, overlooks with river views, an amphitheater and labyrinth, natural stone seating, interpretive signs, benches, trash receptacles, light fixtures, ornamental railing, landscaping, and other design elements. The park should be coordinated with the proposed riverwalk under design and construction by the Rotary Club.
- Construct a riverfront center along the Milwaukee River in the expanded Civic Campus. The riverfront center would provide a place for community events as well as meetings, private parties, dances, reunions, weddings, etc. Demolish the Logemann Community Center after the riverfront center has been completed.
- Provide parking for riverfront park and proposed riverfront center.
- Complete Zone 5 streetscape improvements discussed later in this report.



Proposed Riverfront Park





Riverfront Center Example



Milwaukee River



Community Reader Board at Cedarburg Road and Mequon Road



Proposed Gateway Entrance at Cedarburg Road and Mequon Road



3. Streetscape Zone 1

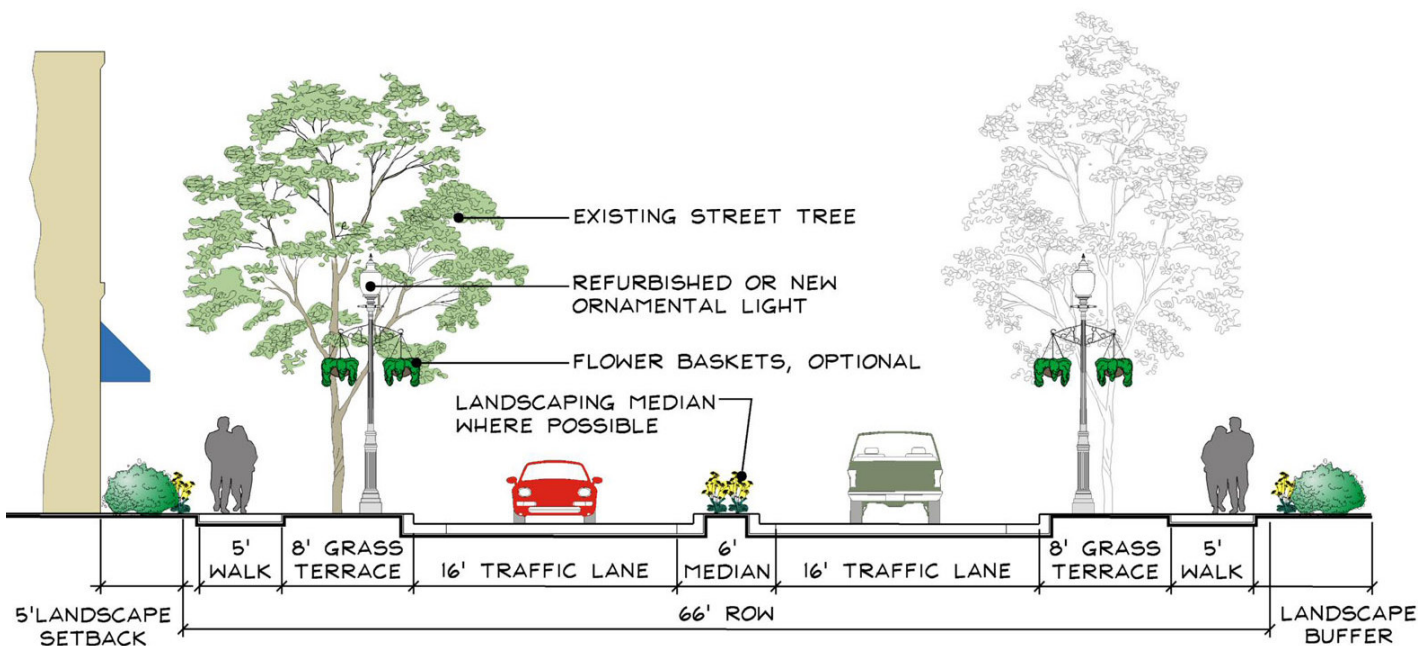


Existing North Gateway

The Zone 1 Streetscape design serves as a transition to the Town Center from the north. Gateway monuments, ornamental lighting, landscaping, landscaped medians and enhanced pedestrian crossings incorporate elements to welcome users to the Town Center. The streetscape design provides a framework for future redevelopment projects that are brought closer to the right-of way to enclose the street. Zone 1 mostly occurs in the Arrival District along North Main Street from Concord Place to Riverview Drive, and



Proposed North Gateway



Streetscape Zone 1 - Typical Section

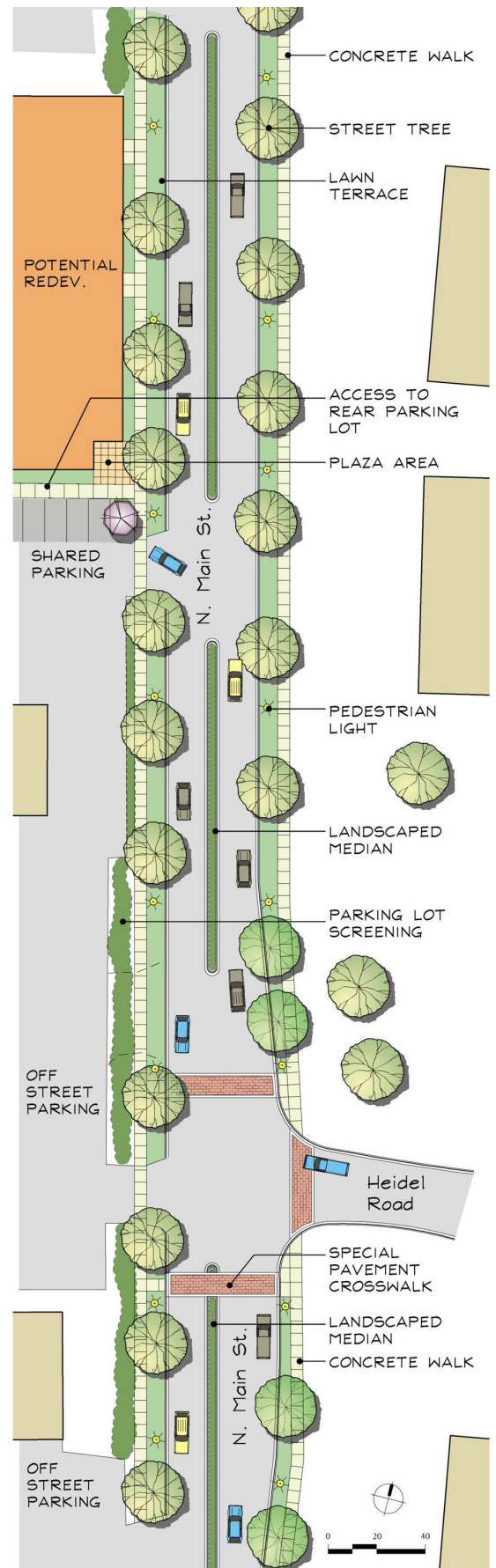


along Freistadt Road and Riverview Drive from North Main Street to Green Bay Road. The following is a list of proposed Zone 1 streetscape improvements.

- Improve existing entrance monuments at Concord Place with additional stone walls, up lighting, landscaping and decorative fire tower cap.
- Maintain existing road pavement and curb and gutter.
- Provide colorful, seasonal landscaping in the existing medians and provide additional medians where possible based on curb cuts and emergency vehicular access.
- Enhance crosswalks with special pavement.
- Eliminate driveways where possible and create shared openings in future developments.
- Provide new streetlights or refurbish existing lights with new luminaries.
- Infill street trees as needed.
- Provide wayfinding signage to Town Center, Village of Thiensville and City of Mequon public and major community destinations. Destinations include Village Hall, Village Park, Library, City Hall, riverfront park, riverfront center, etc.
- Promote internal circulation of parking areas to reduce the number of curb cuts along along the arrival corridors.
- Coordinate private landscaping with the public streetscape to provide screening of parking lots along with color and visual interest.
- Promote streetscape design that incorporates the Historic Character of the Village and Town Center.
- Provide pedestrian safety improvements at intersections such as bump-outs, special pavements and clearly marked crosswalks.



Existing North Main Street (Zone 1)



Streetscape Zone 1- Plan



4. Streetscape Zone 2



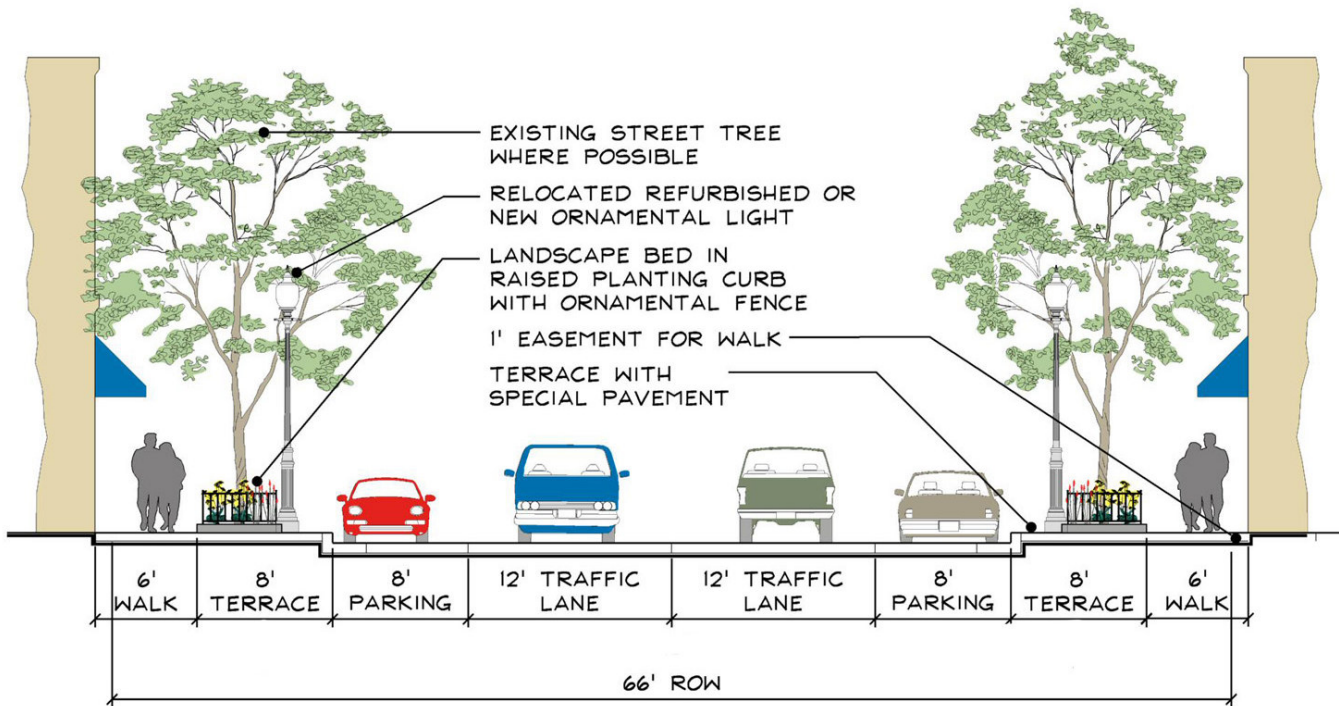
Existing North Main Street



Existing South Main Street

The Zone 2 Streetscape creates a pedestrian friendly environment that serves the Town Center's main commercial corridor. The streetscape uses a variety of design elements, such as decorative lighting, landscaping, special pavement and site furnishing, to create a design based on the historical character of the Town Center. This is the most intensely designed streetscape in the Town Center. Zone 2 is located within the Historic Village and Expanded Village Districts along Main Street from Division Street to Riverview Drive and one block east of Main Street at Buntrock Avenue. The following is a list of proposed Zone 2 streetscape improvements.

- Develop a road cross-section that accommodates a minimum 2 travel lanes and parallel parking on each side of the street similar to Green Bay Road.
- Where possible, provide bump-outs at intersections to provide pedestrian safety.
- Enhance crosswalks with special pavement.
- Reduce the number of driveway openings on Main Street between Division Street and Freistadt Road.
- Provide new streetlights or refurbish existing lights with new luminaries.



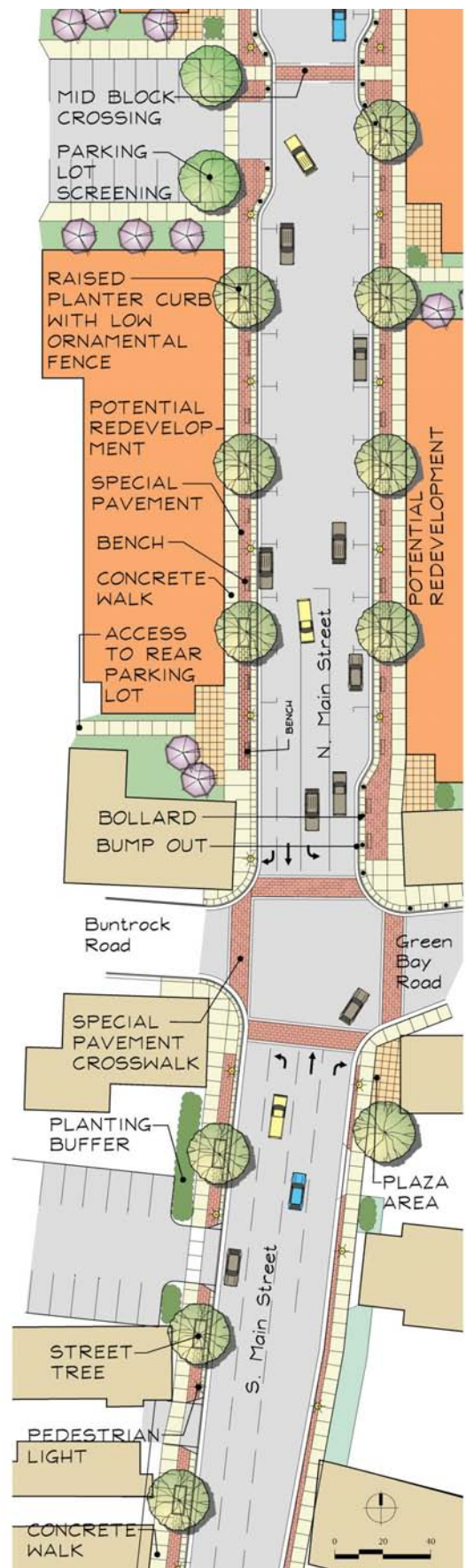
Streetscape Zone 2 - Typical Section



- Attempt to work around existing street trees.
- Construct curbed planting beds with ornamental fencing to protect trees and provide space for future growth and improved survivability. Provide additional landscaping in planters with shrubs, perennials and annuals.
- Provide sidewalks from back of curb to right-of-way that include a combination of brick pavers and concrete pavement
- Provide historic period style benches, trash receptacles, planters, bollards, etc.
- Provide informational kiosks with Town Center maps to assist in visitor orientation and provide poster case for additional information about community events.
- Implement wayfinding sign system.
- Improve entrance monuments at Division Street with additional stone walls, lighting, landscaping and fire tower motif.
- Coordinate private landscaping with the public streetscape to provide screening of parking lots along with visual interest and color.



Existing South Gateway Feature



Streetscape Zone 2 - Plan



5. Streetscape Zone 3



Existing Zone 3 Streetscape -
Green Bay Road

The Zone 3 Streetscape design connects the Village Municipal Building and Village Park to the businesses along Green Bay Road. The zone also provides a transition between the businesses on the south end of Green Bay Road with residential land uses to the north. This zone occurs within the Historic Village and Arrival Corridor Districts along Elm Street between Green Bay Road and the Village Park, and along Green Bay Road from Riverview Drive to Freistad Road. The following is a list of proposed Zone 3 streetscape improvements.

- Provide new streetlights or refurbish existing lights with new luminaries.
- Infill street trees as needed.
- Implement wayfinding system to community destinations.

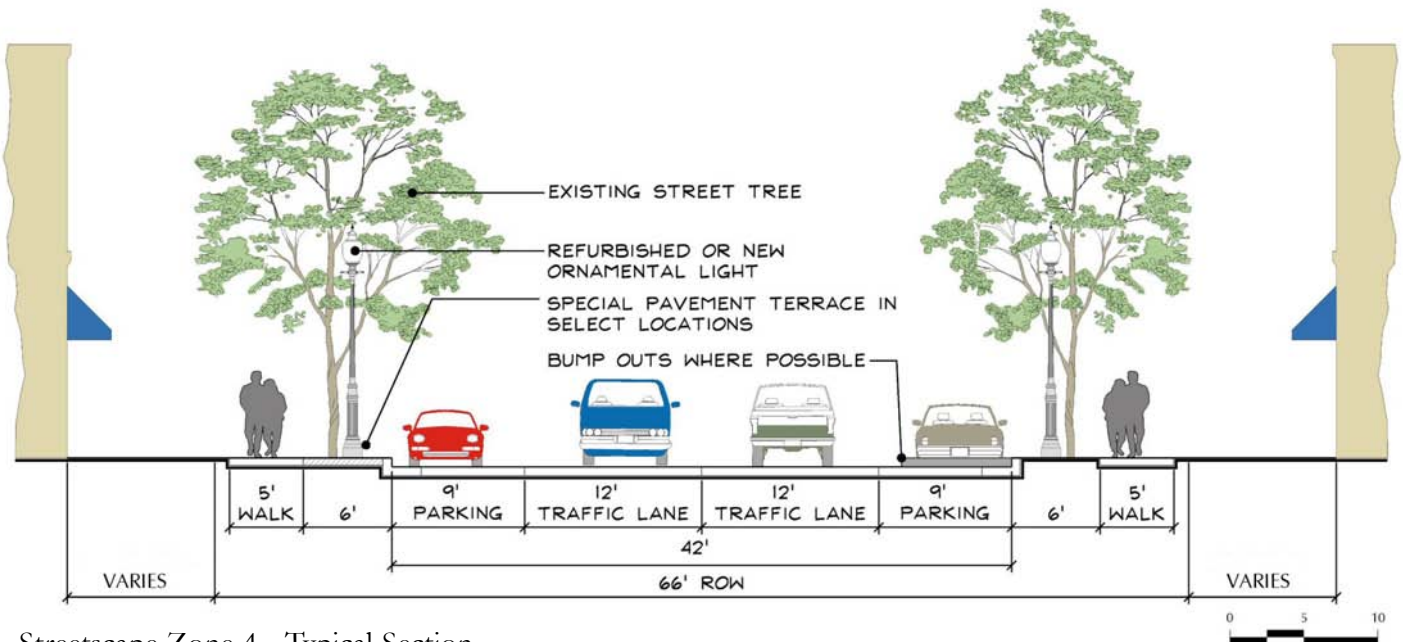


Existing Zone 4 Streetscape-
Green Bay Road

6. Streetscape Zone 4

The Zone 4 Streetscape occurs in the heart of the Historic Village along Green Bay Road from Buntrock Rock to Riverview Drive. Improvements in this zone focus on maintaining the existing character of the streetscape. The following is a list of proposed streetscape improvements.

- Where possible , provide bump-outs at intersections to provide pedestrian safety.



Streetscape Zone 4 - Typical Section



- Provide new streetlights or refurbish existing lights with new luminaries.
- Infill street trees as needed.
- Install brick pavers at selected locations.
- Provide historic period style benches, trash receptacles, planters, bollards, etc.
- Implement wayfinding sign system.
- Improve connection to Village Municipal Building by enhancing Elm Street intersection with bump-outs, and providing improvements to directional signage and landscaping.
- Enhance crosswalks with special pavement.



Enhanced Pedestrian Crosswalk Example



Existing Directional Sign to Village Hall



Streetscape Zone 4 - Plan



7. Streetscape Zone 5



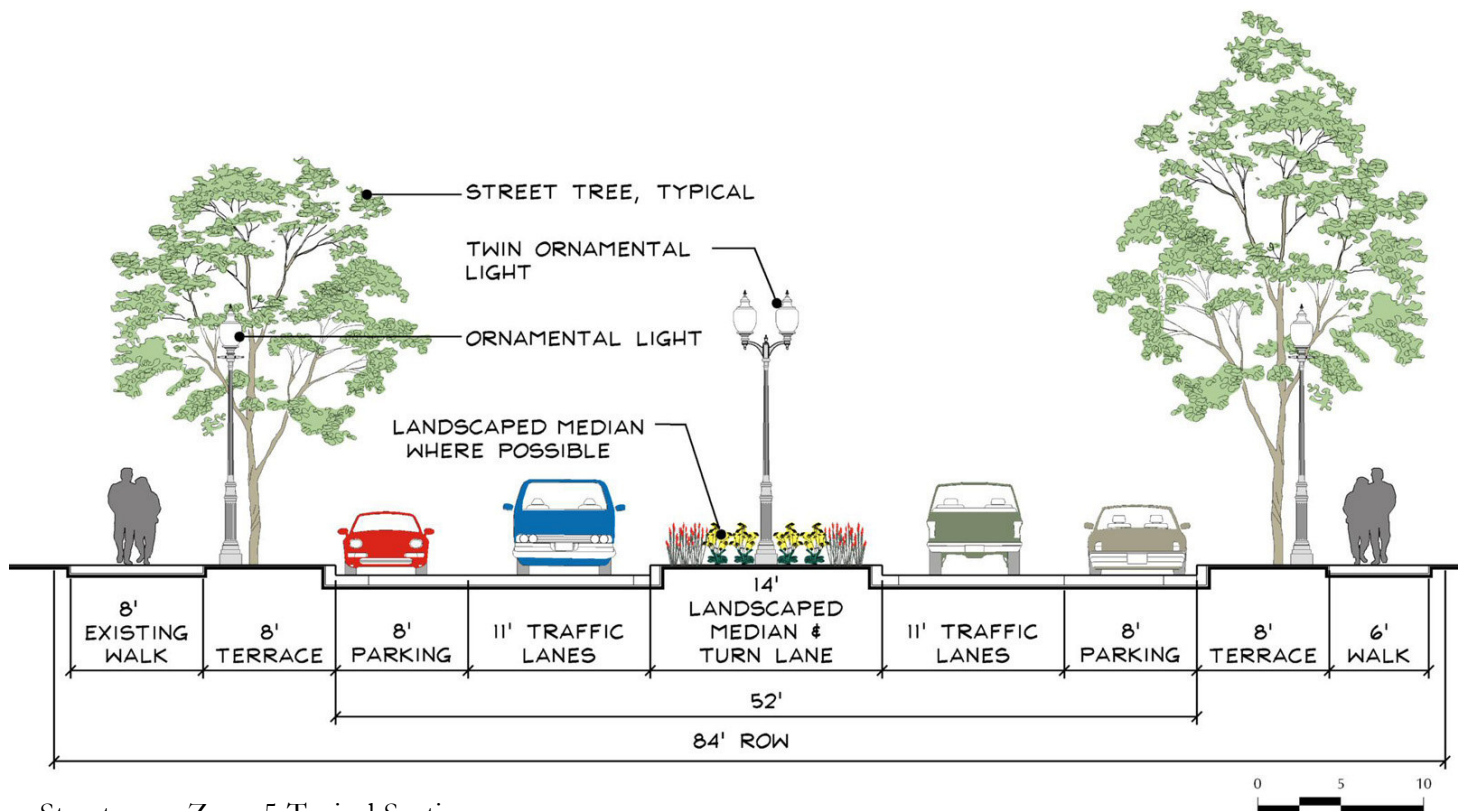
Cedarburg Road - Looking South



Cedarburg Road - Looking North

The Zone 5 Streetscape runs through the Civic Campus District and separates the existing campus from the proposed riverfront park. This streetscape provides a connection between these two major public areas. Landscape medians and special lighting are key features for this zone. A major mid-block pedestrian crossing with bump-outs connects the civic campus that is separated by Cedarburg Road. Zone 5 occurs along Cedarburg Road from Mequon Road to Division Street. The following is a list of proposed streetscape improvements.

- Reconstruct Cedarburg Road to have the following:
 - o Sidewalks and grass terraces on both sides of road
 - o Curb and gutter on both sides of road
 - o Two travel lanes, one in each direction
 - o Parallel parking on both sides of road
 - o Landscape medians with ornamental lights
- Provide streetscape design that incorporates the historic character of the Village and Town Center.



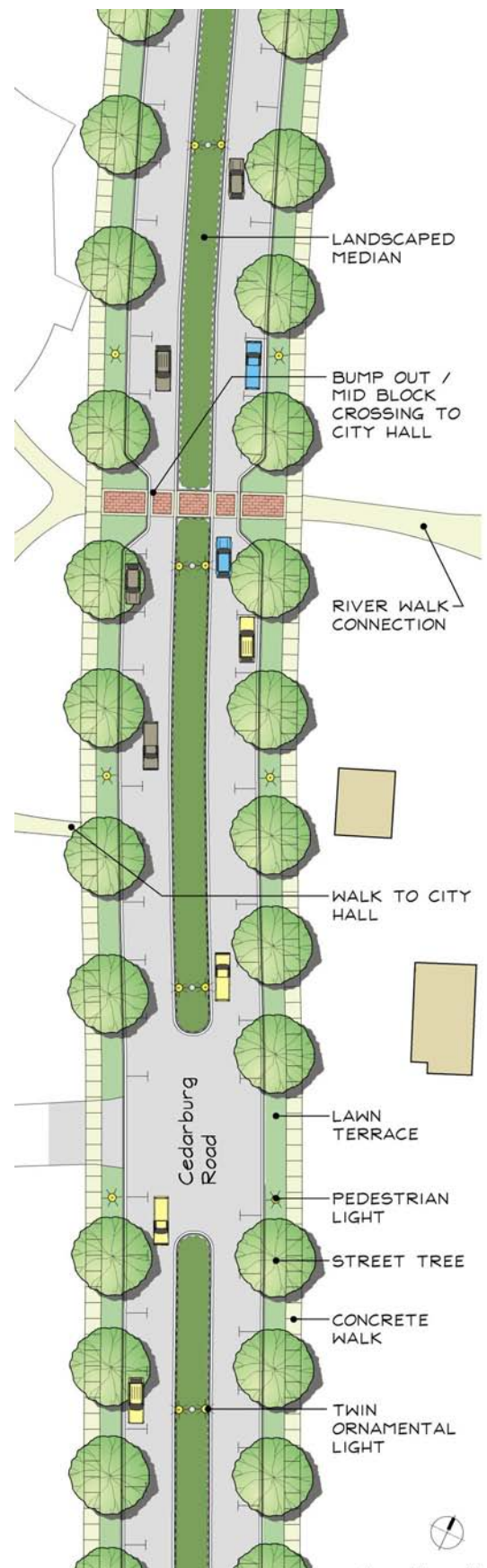
Streetscape Zone 5 Typical Section



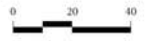
- Bury overhead utilities along Cedarburg Road from south of Mequon Road north through the Civic Campus Design District
- Connect the existing Civic Campus to the proposed river-front park with a mid-block table-top crossing that includes bump-outs and special pavement.
- Enhance crosswalks with special pavement.
- Light the roadway with a combination of single-fixtured pedestrian scale lights in the terrace and twin-fixtured pedestrian scale lights in the median.
- Install street trees in a formal pattern along road.
- Promote and implement attractive wayfinding and regulatory signage.
- Enhance medians with landscape plantings that include low shrubs, perennials, and grasses.



Proposed Civic Campus Plan



Streetscape Zone 5 - Plan



8. Streetscape Zone 6



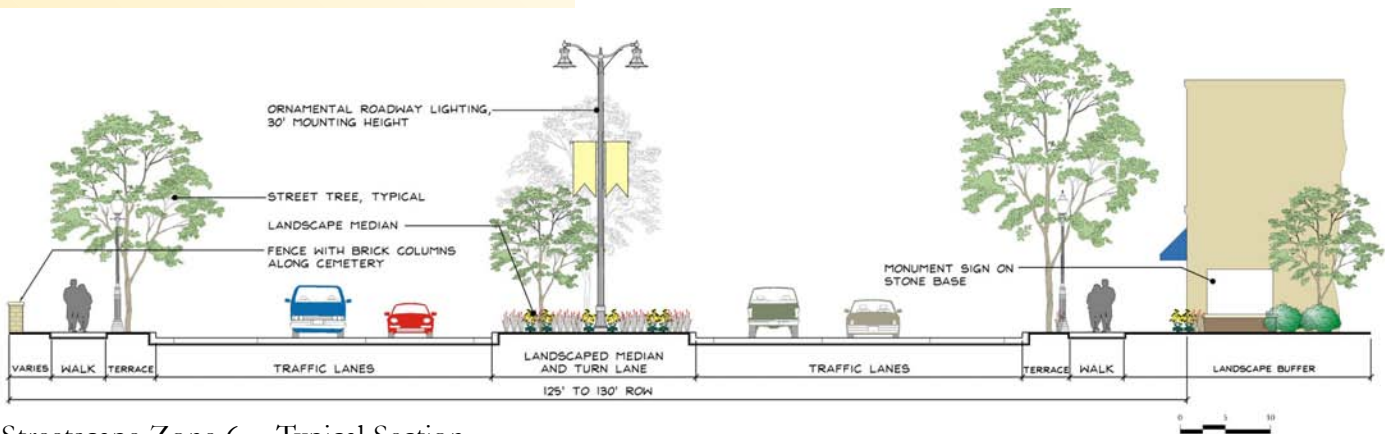
Mequon Road



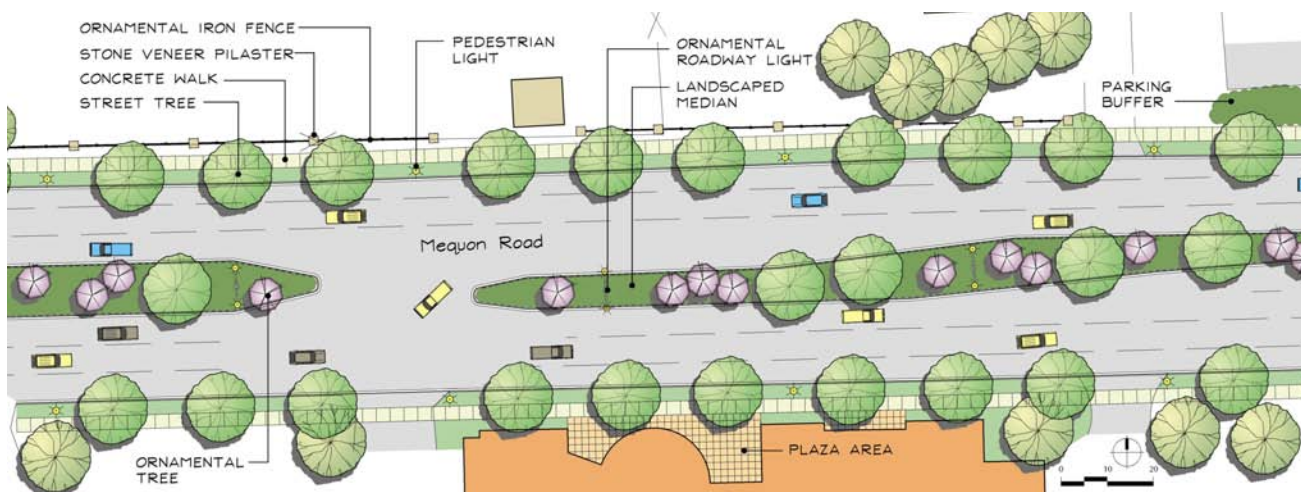
Intersection of Cedarburg Road and Mequon Road

The Zone 6 Streetscape comprises the main east/west corridor for the Town Center and has mostly vehicular orientated businesses. This zone occurs in the Arrival Corridor District along Mequon Road from the Milwaukee River Bridge to Buntrock Avenue. The existing roadway remains unchanged with its two travel lanes in each direction, right turn/break-down lanes and wide median. Recommendations to this zone include improvements to the Milwaukee River Bridge, landscaping, lighting and wayfinding signage. The following is a list of proposed streetscape improvements.

- Provide streetscape design that incorporates the historic character of the Village and Town Center.
- Light the roadway with a combination of pedestrian scale lights in the terrace and ornamental roadway lights in the median.
- Enhance crosswalks with special pavement.
- Install street trees in a regular pattern along road.



Streetscape Zone 6 - Typical Section



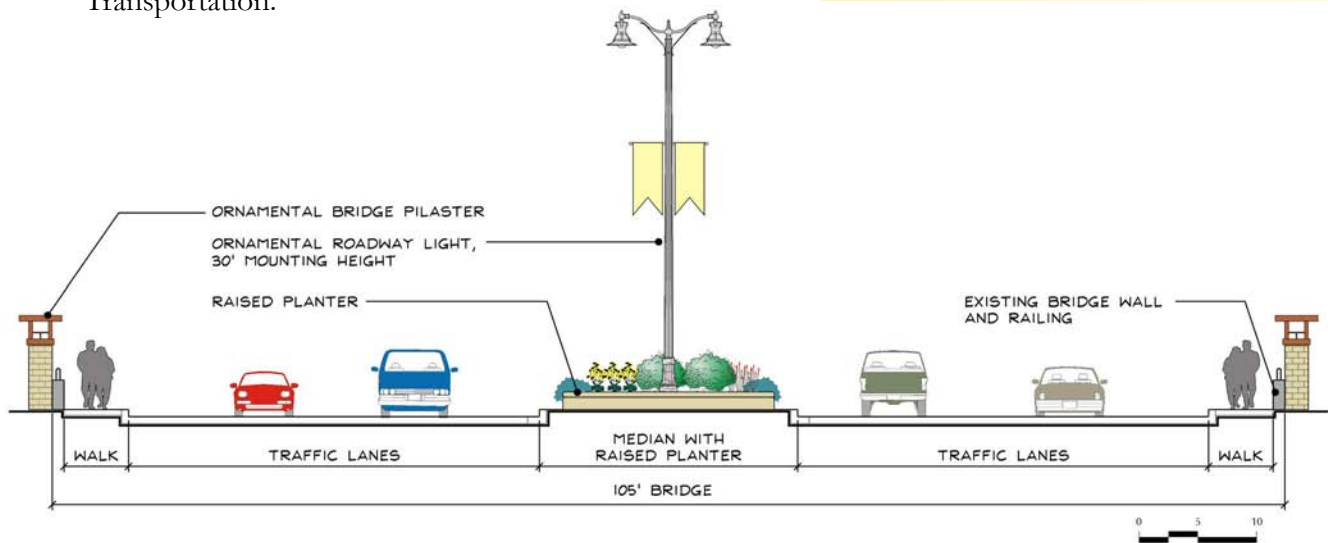
Streetscape Zone 6 - Plan



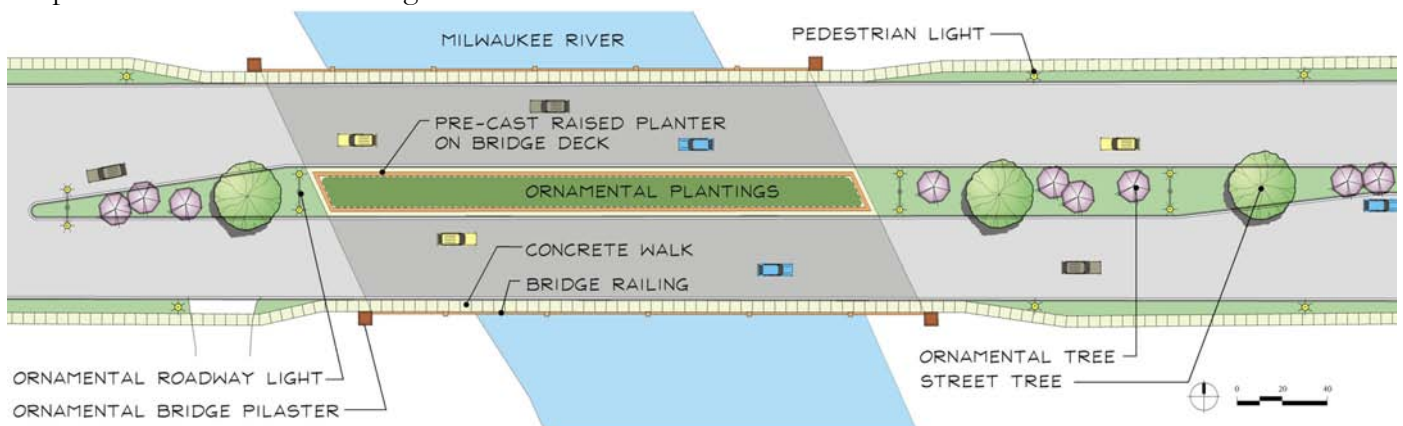
- Improve the perimeter of the cemetery with brick/stone columns, ornamental wrought iron fencing, and gateway entrances that face Mequon Road.
- Plant shade and ornamental trees in the medians along with ornamental shrubs, perennials and grasses.
- Enhance Milwaukee River Bridge to create a gateway for the Town Center by including the following:
 - Construct a raised planter in the median.
 - Provide ornamental bridge pilasters at the ends of the bridge. The pilasters are not connected to the bridge.
 - Upgrade to decorative design or paint existing bridge railing.
 - Provide ornamental roadway lighting in the median.
 - Light the sidewalks approaching the bridge with pedestrian scale lights.
 - Plant shade and ornamental trees in the medians on both ends of the bridge.
 - All proposed improvements to the bridge will require review and approval by the Wisconsin Department of Transportation.



Milwaukee River Bridge



Proposed Milwaukee River Bridge - Section



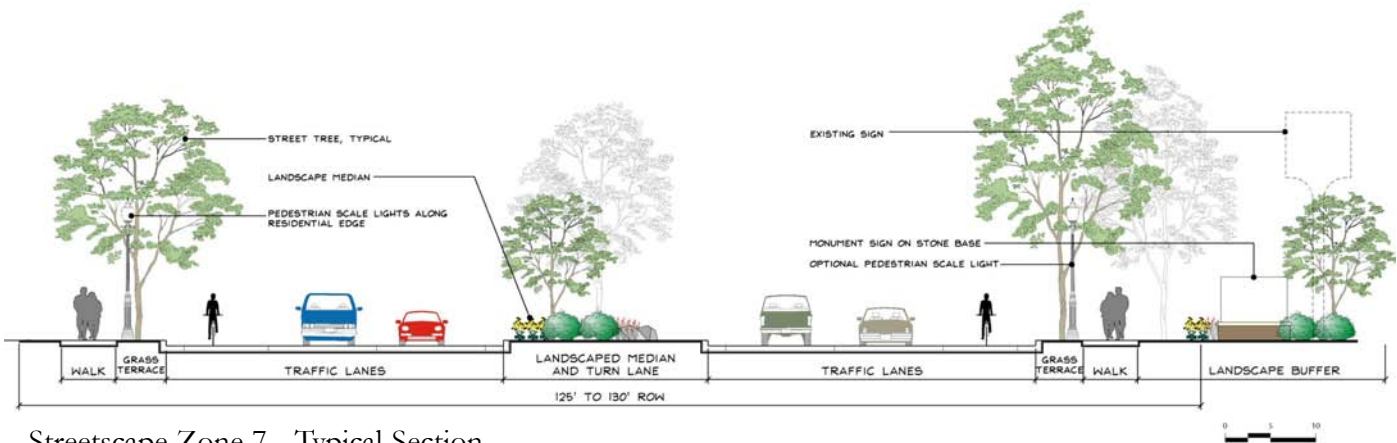
Proposed Milwaukee River Bridge - Plan



9. Streetscape Zone 7

Although technically outside of the Town Center, the Zone 7 Streetscape serves as a transition from the Town Center to outlying areas to the west and conversely, as a gateway to the Town Center. Compared to the formal landscape pattern in Zone 6, Zone 7 has an informal pattern to better transition into the surrounding countryside. The existing roadway remains unchanged with its two travel lanes in each direction, right turn/breakdown lanes and wide median. The improvements for this zone include landscaping and lighting. Zone 7 occurs along Mequon Road from Buntrock Avenue to Wauwatosa Road. The following is a list of proposed streetscape improvements.

- Provide streetscape design that incorporates the historic character of the Village and Town Center.
- Provide pedestrian scale lights in the terrace to light the sidewalk.
- Install combination of street and ornamental trees in an informal pattern along road.
- Plant shade and ornamental trees in the medians.
- Enhance medians with pockets of landscape plantings that include low shrubs, perennials, grasses and boulders.



Streetscape Zone 7 - Typical Section

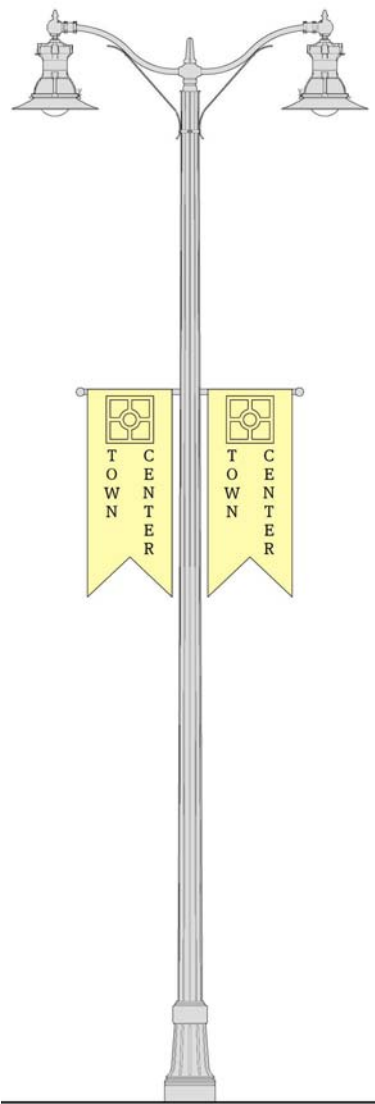


Streetscape Zone 7 - Plan



10. Streetscape Amenities

As described above and in the Design Guidelines, the Town Center encompasses a variety of areas, each with their own distinct feel. A successful design for the Town Center must unite these parts into a unified whole. One way to do this is to use a coordinated family of streetscape and site amenities, such as lighting, pavement materials, benches, trash receptacles, bollards, floral planters, hanging flower baskets, signage, etc. Working in concert with the Design Guidelines, the selection of amenities for the streetscape zones will create a unified historic theme for the Town Center.



Roadway Light



Town Center Light



Bollards

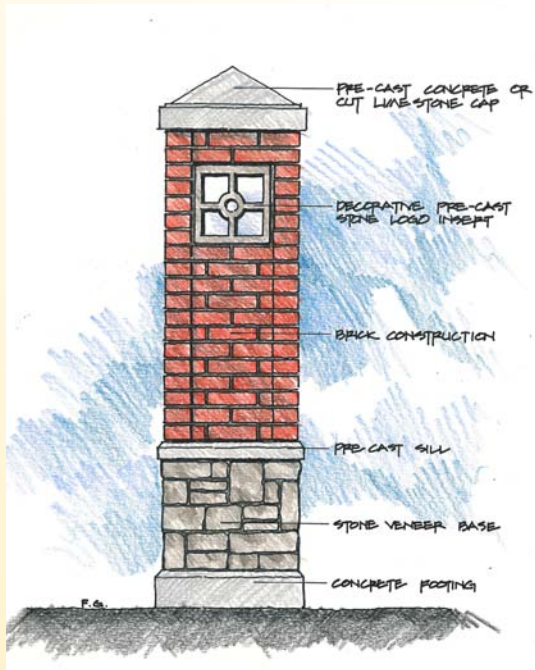


Hanging Floral Baskets

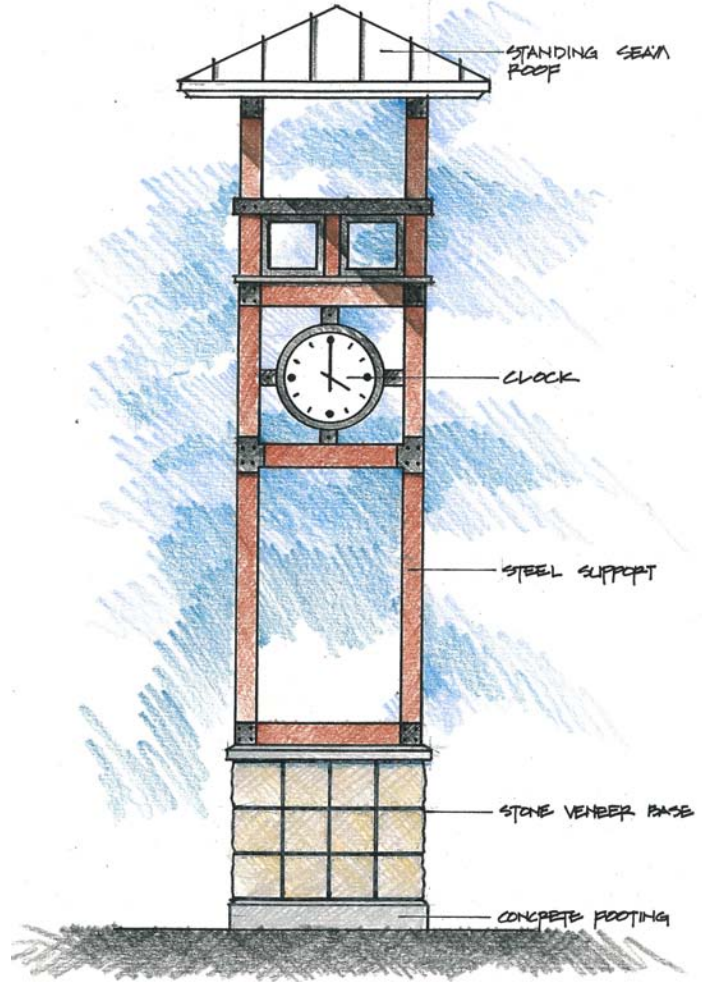


Floral Planters

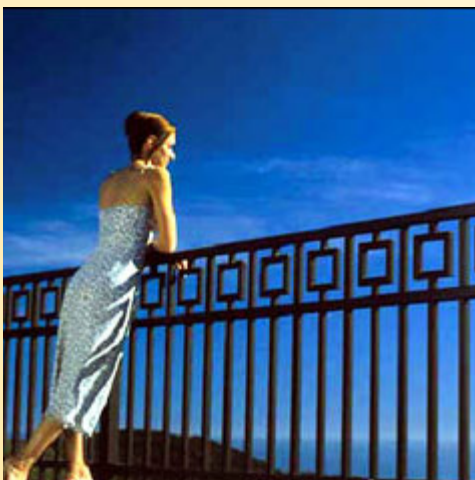




Decorative Column



Clock Tower



Decorative Fence/Railings



Decorative Fence with Columns



Enhanced Crosswalk and Brick Pavers



Donor Pavers



Paver Sidewalk and Planter Curb



Donor Pavers



Planters with Ornamental Fencing



Brick Pavers





Trash Receptacles



Interpretive Sign



Wayfinding Sign



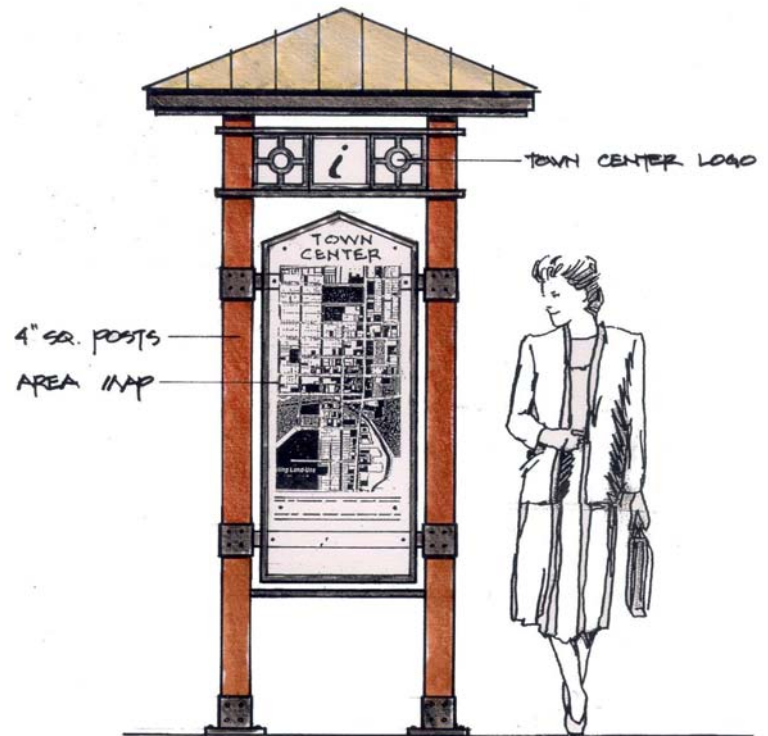
Signage



Bicycle Rack



Bench



Informational Kiosk

